

United States Department of the Interior
National Park Service

National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions.

1. Name of Property

Historic name: Deary Garage

Other names/site number: Beyer Motor Co.; Deary Service Station; The Pie Safe Bakery

Name of related multiple property listing: N/A

(Enter "N/A" if property is not part of a multiple property listing)

2. Location

Street & number: 307 Main Street

City or town: Deary State: Idaho County: Latah

Not For Publication: Vicinity:

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

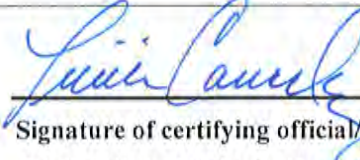
I hereby certify that this X nomination ___ request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property X meets ___ does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance:

___ national ___ statewide X local

Applicable National Register Criteria:

X A ___ B X C ___ D

 Signature of certifying official/Title: <u>Tricia Canaday, Deputy SHPO</u>	<u>12-15-23</u> Date
<u>Idaho State Historic Preservation Office</u> State or Federal agency/bureau or Tribal Government	

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In my opinion, the property ___ meets ___ does not meet the National Register criteria.

Signature of commenting official: _____ **Date** _____

Title: _____ **State or Federal agency/bureau or Tribal Government** _____

4. National Park Service Certification

I hereby certify that this property is:

- ___ entered in the National Register
- ___ determined eligible for the National Register
- ___ determined not eligible for the National Register
- ___ removed from the National Register
- ___ other (explain:) _____

Signature of the Keeper

Date of Action

5. Classification

Ownership of Property

(Check as many boxes as apply.)

- Private:
- Public – Local
- Public – State
- Public – Federal

Category of Property

(Check only **one** box.)

- Building(s)
- District

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Site

Structure

Object

Number of Resources within Property

(Do not include previously listed resources in the count)

Contributing	Noncontributing	
<u>1</u>	<u> </u>	buildings
<u> </u>	<u> </u>	sites
<u> </u>	<u> </u>	structures
<u> </u>	<u> </u>	objects
<u>1</u>	<u> </u>	Total

Number of contributing resources previously listed in the National Register 0

6. Function or Use

Historic Functions

(Enter categories from instructions.)

Commerce/Trade: specialty store

Current Functions

(Enter categories from instructions.)

Commerce/Trade: restaurant

Commerce/Trade: specialty store

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7. Description

Architectural Classification

(Enter categories from instructions.)

Other: No style

Materials: (enter categories from instructions.)

Principal exterior materials of the property:

foundation: Concrete

walls: Brick

roof: Asphalt

other: garage doors: Wood Metal

Narrative Description

(Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable. Begin with a **summary paragraph** that briefly describes the general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity.)

Summary Paragraph

The Deary Garage is a one-story, 7,500 square foot commercial vernacular building with a rectangular footprint but for a slightly curved, front entry façade at the corner facing the intersection of Main Street and 2nd Avenue in central Deary. The building has a flat roof and a low parapet with a metal coping. Overhead garage doors that once allowed for driving through the building are located on the west and east elevations. The brick masonry building has combed brick veneer on the exterior and diamond-shaped brick embellishments in the brick-clad panels that surmount the east elevation, as well as the main entry at the top above the storefronts. The corners of the building that frame the front entry feature a brick dogtooth pattern at the corners. The building has a concrete foundation and a built-up roof. The Deary Garage was constructed in 1926. It is significant at the local level and retains integrity of location, design, setting, materials, workmanship, and feeling.

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Narrative Description

Location and setting

The Deary Garage is located in Deary, Idaho, a town in Latah County in northern Idaho that is approximately 25 miles east of Moscow, Idaho, the county seat, and 124 miles west of Missoula, Montana. It abuts the Saint Joe National Forest to the north and the Palouse Range to the west. The Deary Garage is located just north of 2nd Avenue or Highway 8, the main route that travels east-west through the town and just west of Main Street, a major street traveling north-south through town. The former Washington, Idaho and Montana railroad tracks, whose bed is still visible on the landscape, entered the town from the northwest and continued east parallel to Highway 8 after Wyoming Street. The small town has a population of approximately 500 people.

The Deary Garage is located within a sparsely developed commercial strip that stretches along Main Street. Adjacent to and directly north of the Deary Garage is the newer Deary Fire Station, a tall one-story building. Beyond that is a one-story commercial building that houses the Adventist Community Services Food Distribution Center. There is a small break in the block face at this point, which takes up again with the Deary Mercantile that is among a small cluster of late 19th century/early 20th century, one-and-two-story commercial buildings that are all party wall structures. On the other side of the block is a gabled building to the south that has a colorful painted sign that says, "Greetings from Deary." To the north is a city park. The remainder of the commercial structures along 2nd Avenue are primarily modest, freestanding buildings. The residential areas are for the most part located to the north and northeast of Deary's downtown.

The Deary Garage, a largely rectangular building, faces southeast, toward the intersection of Main Street and Highway 8, or 2nd Avenue. The angled entry to the building faces this intersection, while the side elevations of the building face these two primary streets. A landscaped strip and sidewalk border the south side of the building along 2nd Avenue, while the east side of the building abuts the sidewalk along Main Street. A parking area and loading zone is located in front of the west elevation or rear of the building. Landscape features are as follows. A small, raised patio is located at the entry to the building. The retaining wall for the patio is faced with stone, bordered with plantings at the outside edge, and accessed via three concrete steps from the corner. A deep planting bed of varying heights is located along the south edge of the building and partially screens the work area on the west side. The work area on the west side of the building consists of two concrete pads and is accessed via a short, graveled alley. The east side of the building is located adjacent to the concrete sidewalk and displays no landscaping. A lowered concrete walkway with a concrete retaining wall accesses the pedestrian entry here.

Overview

The Deary Garage is a one-story, 7,500 square foot commercial building with a rectangular footprint but for a slightly curved, front entry façade at the corner facing southeast towards the intersection of Main Street and 2nd Avenue. The building has a flat roof with a few skylights, a very low parapet, and a metal coping. It is located at 307 Main Street and faces southeast,

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overlooking the intersection of these two downtown streets in central Deary. Overhead garage doors that once allowed for driving through the building are located on the west and east elevations. The building forms a party wall with the adjacent metal-clad Deary Fire Station to the north. The brick masonry building has combed brick veneer with running bond on the exterior and diamond-shaped brick embellishments in the brick-clad panels that wrap around the southeast-facing façade and east elevation at the top above the storefronts. The corners of the building that frame the front entry feature a brick dogtooth pattern at the corners. The building has a concrete foundation and a built-up roof. It is a vernacular commercial building constructed in 1926.

Exterior

Southeast (entry) façade: The building's façade is on the angled, slightly concave curved corner that faces southeast towards the intersection of Main Street and 2nd Avenue. It consists of a centered wood entry door with full-height glass flanked by wood-framed sidelights, also with full-height glass. Above this ensemble is a five-light transom window with narrow muntins. Large, single-light plate-glass windows flank the front entry, each with a low sill and topped by a six-light transom, also with narrow muntins. Above the entry door is a (replacement) flat canopy supported by two wood posts.

East elevation: The east elevation faces onto Main Street and features tall, paired, plate glass windows at the corner or south end, topped by six-light transoms. Adjacent to these windows is an overhead door that once served as an automobile entry door. Today the sloped street outside this door is built up such that the entry walkway outside the door is below the public sidewalk and protected by an open metal fence. The overhead garage door is modern, with multiple lights. To its right is a pedestrian entry door with full-height glass, topped by two transom windows. Adjacent to this, at the north building corner, is a large, single-light, plate-glass window. Industrial lights surmount the windows.

South elevation. The south side elevation faces 2nd Avenue. It is composed of an original four-panel, folding wood door with four, three-light windows above solid panels of diagonally placed wood. Windows on this elevation consist of five paired, six-over-six-light, double-hung windows. These have molded aluminum frames on the exterior and wood on the interior, and reflect the design, configuration, and operation of the original windows. Industrial lights surmount the windows.

West (rear) elevation. This elevation faces onto a utility area. It features two paired, six-over-six-light windows that are similar in design to the ones on the south elevation. A contemporary overhead garage door composed of panels and five largely centered lights is close to the northwest corner of the building. There are two pedestrian doors on this façade as well. The finish here is stucco.

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Interior

The interior of the building is described below in three bays, running south to north, and is identified as the east bay, the central bay, and the west bay. The entry room is included in the east bay.

East bay. The south end of the east bay features a seating area for the restaurant/bakery inside the main entrance. The main sales area and a pizza oven located within an arched brick opening that originally was the garage's bank vault are straight ahead to the north. The north end of the east bay contains a larger dining area, accessed through a broad opening from the entrance seating area. This dining area, originally a garage bay, also occupies the north portion of the central bay and terminates at a hallway (to the west) that leads to the restrooms and offices in the northwest corner of the building.

The entry area has a fir floor and pressed tin ceiling (both likely original). The larger dining room has a floor of (original) polished concrete, (original) exposed brick walls, and an (original) unfinished ceiling with exposed open timber trusses supported by timber posts.

Central bay. The center bay of the building contains the gift shop (at the south end) and support spaces, including the kitchen for the bakery in the center of the building and beyond, one of the restrooms off the east-west hall. The gift shop has an (original) board ceiling, board-clad partitions, and an (original) fir floor.

West bay. The west bay of the building contains the cheese kitchen on the south end. (It is necessary to keep the cheese kitchen separate from the pizza and sandwich kitchen in the central bay due to the different yeasts and food stuffs being used). Beyond that are support spaces, including three walk-in coolers, a mechanical room, and a storage area, and the second restroom and an office in the northwest corner.

Change Over Time

The Deary Garage functioned as an automobile garage and showroom during the 1926-1945 period of significance and underwent changes typical of its building type to meet the evolving needs of the automobile. Historic photographs document early exterior alterations. A photograph that was likely taken about the time the building opened shows that at that time the columns supporting the canopy roof consisted of tapered square posts mounted on low brick piers; the canopy roof eaves were supported by block modillions. The gas pump located between the columns was an earlier style gas pump. The front entry door had three-light sidelights over a wood panel. At the south end of the east elevation was a pedestrian door adjacent to the paired plate glass window in this location. The original garage door on the east elevation was in place and was similar to the garage door that is on the south elevation. It consisted of a double-leaf hinged door with nine lights in the upper portion of each leaf, with a small projecting sill and wood panel below. By the late 1940s/early 1950s, two modern gas pumps had replaced the earlier gas pump and simple wood posts replaced the original canopy supports. This appearance was maintained in the 1950s.

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The present owners bought the building in 2015 for rehabilitation into a bakery and restaurant, at which time the building had undergone few alterations since the period of significance and it was still occupied by cars. Its most recent use had been as a Ford dealership. Known changes prior to 2015 include the removal of the canopy and gas pumps at the main entrance. It is not known when the first pedestrian door on the east elevation was removed, but it was sometime between the 1950s and 2015. The walkway to the pedestrian entry on the east side of the building originally followed the grade of the sidewalk, as did the entry. At an unknown date both areas were raised as the grade of the surrounding roads changed.

Exterior changes (since 2015) have consisted primarily of maintenance and projects that replaced features in kind. In 2015 the roof was redone, and a few skylights added. The bricks were repointed. The windows were replaced in kind, with windows in the same location with the same configuration and operation as they had in the recent past. The profiles on the muntins also reflect the original profiles. The canopy, missing in a 2008 photograph, has been reinstated.

The interior of the building was reconfigured to serve as a bakery and restaurant, but it retains the overall spatial qualities of its original use as an automobile garage. Throughout, the building systems were replaced and upgraded when the rehabilitation of the building was undertaken in 2015. Known alterations to each section of the interior are as follows:

East bay. The entry area retains the spatial qualities of the original automobile supplies store. It also retains the original vault area, which is used to house a pizza oven. Original trusses have been retained on the far east side and northeast sides of the building, but intermediate timber posts have been added for additional support for what was originally a 45' span. The overhead garage door is new but in its original location. Original materials and finishes have been retained. The area in the northeast corner originally housed automobiles in the garage.

Central bay. New features in the central bay include the configuration of the support spaces. Original materials include the board ceiling and board partitions. The southerly exposed brick wall is original.

West bay. The west bay has been reconfigured for support spaces. Many of the partitions in the support spaces are new and are finished in drywall. However, an original four-panel wood door leading to 2nd Avenue on the south façade is original. The overhead garage door on the west façade is new but is located within the original opening.

Integrity

Responses to the following aspects of integrity will determine whether the Deary Garage retains integrity and is therefore a historic property and eligible for listing in the National Register of Historic Places. A resource must be significant, as determined in response to its historic context, to be considered eligible for listing in the National Register. That is, a property must represent a significant part of the history, architecture, archeology, engineering, or culture of an area and must have the characteristics that make it a good representative of properties associated with

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that aspect of the past.¹ Additionally, it must be over 50 years old in most cases and must retain integrity.² Significance can be at the local, state, or national level. In the case of the Deary Garage, its significance is at the local level. Integrity is the ability of a property to convey its significance. To retain historic integrity, a property will always possess several, and usually most, of the aspects of integrity. It should also retain those aspects that are most important to its significance.³

- Location is the place where the historic property was constructed or the place where the historic event occurred.

The garage is located where it was originally.

- Design is the combination of elements that create the form, plan, space, structure, and style of a property.

The garage conveys its original design. The exterior envelope displays the same form as it did historically. Some exterior features have been replaced in kind. The diamond-shaped decorative features at the top of the building are intact. The most noticeable feature that was changed between about 1926 and the 1940s was the front canopy and supports, which may have been altered to allow for more room for the two new gas pumps. There was no canopy over the entrance in 2008 but by 2021 the present canopy and supports were in place; they may have been added with the 2015 remodel. On the interior some partitions have been added and the structure has been reinforced. The interior entry space retains its original materials and spatial qualities. A pole sign has been removed and exterior lighting (not original) has been replaced with new exterior lighting.

- Setting is the physical environment of a historic property.

The setting of the building has changed over time but is sufficiently intact to convey this aspect. The strong orientation of the building toward this important intersection in downtown Deary is intact. The block face within which the building is located is largely intact to its appearance after the 1923 fire. The city park has replaced the buildings across the street on the north end of the block. A small, raised patio at the entry and perimeter landscaping date to ca 2015.

- Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property.

¹ Patrick W. Andrus, *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*. Washington DC: US Department of the Interior, National Park Service, Cultural Resources, 1995:7.

² Andrus, 1995:41

³ Andrus, 1995:44.

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The materials on the exterior and interior of the building remain largely the same. The rear (west) elevation, which was brick, has been finished with stucco. Interior finishes remain the same with the exception of the new sheet rock partition walls.

- Workmanship is the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory.

Workmanship remains the same. Double-hung windows with divided lights and plate glass windows have been replaced in kind.

- Feeling is a property's expression of the aesthetic or historic sense of a particular period of time.

The property retains its historic feeling.

- Association is the direct link between an important historic event or person and a historic property.

The association has been lost, although both the garage and the restaurant are both service oriented commercial businesses. But the historic association between the modern garage and the auto-driving public has been lost.

In summary, the Deary Garage retains integrity of location, design, setting, materials, and workmanship. It does not retain integrity of association.

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8. Statement of Significance

Applicable National Register Criteria

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- A. Property is associated with events that have made a significant contribution to the broad patterns of our history.
- B. Property is associated with the lives of persons significant in our past.
- C. Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- D. Property has yielded, or is likely to yield, information important in prehistory or history.

Criteria Considerations

(Mark "x" in all the boxes that apply.)

- A. Owned by a religious institution or used for religious purposes
- B. Removed from its original location
- C. A birthplace or grave
- D. A cemetery
- E. A reconstructed building, object, or structure
- F. A commemorative property
- G. Less than 50 years old or achieving significance within the past 50 years

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Areas of Significance

(Enter categories from instructions.)

Commerce

Architecture

Period of Significance

1926-1945

Significant Dates

1926 – Construction date

1945 – Death of L.O. Beyer

Significant Person

(Complete only if Criterion B is marked above.)

N/A

Cultural Affiliation

N/A

Architect/Builder

Unknown

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Statement of Significance Summary Paragraph (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations.)

The Deary Garage is significant under National Register of Historic Places (NRHP) Criteria A and C, at the local level of significance. It is eligible under Criterion A in the area of COMMERCE for its association with Deary's transition from its beginnings as essentially a railroad town to a town that catered, through this building, to the motoring public as automobile ownership rose and the state highway system, along which the property is located, improved. The Deary Garage was the town's first modern, purpose-built, all-inclusive garage, with automobile repair, auto supplies sales, and gasoline sales for the motorist. Built in the aftermath of a 1923 fire that burned most of the business district, the garage also represents Deary's commercial development during the ensuing period of reconstruction. The Deary Garage is eligible under Criterion C in the area of ARCHITECTURE for its design, as a notable local example of early automobile-oriented architecture and as an attractive local commercial vernacular building that is representative of its time and retains integrity. The period of significance is 1926-1945, beginning with the building's completion and ending with the death of Leo O. Beyer, who founded the Deary Garage.

Narrative Statement of Significance (Provide at least **one** paragraph for each area of significance.)

HISTORIC CONTEXTS

Latah County

The Nimiipuu (Nee-Mee-Poo), or Nez Perce, a tribe of the Sahaptian language family, were the first people to call what is now Latah County home. Two ancient Native American north/south trails, the Greater Nez Perce Trail and the Red Wolf Trail, passed through the region about two miles northeast of present-day Moscow, facilitating access to the area.⁴ The first visitors of European-American descent to reach the region were fur trappers, followed by missionaries in the 1830s. Territorial Governor Isaac I. Stevens drew up the first treaty with the Nez Perce and other Native Americans in the area in 1855, preceding the discovery of gold in the region. The second treaty, which reduced the Nez Perce's reservation land by 90%, dates to 1863, after the discovery of gold on the original reservation land.⁵ The last major clash between the Nez Perce and the US Army occurred in 1877.

The area that became Latah County was known for mining, beginning with the discovery of gold by Elias D. Pierce in 1860.⁶ In 1862, the earliest placer mining activity in the area occurred with the establishment of mines along some watercourses. Miners obtained gold, silver, mica, and

⁴ Julie R. Monroe, *Latah County*. Charleston, SC: Arcadia Publishing, 2006:7.

⁵ "Nez Perce National Historical Park, ID, MT, OR, WA, *National Park Service*, <https://www.nps.gov/nepe/learn/historyculture/the-treaty-era.htm>, accessed May 2023.

⁶ Dulce Kersting-Lark, "Nearby Historic: A history of mining in Latah County, *Moscow-Pullman Daily News*, February 15, 2016.

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opals in the area, primarily in the Hoodoo district, which was located in what is today northeast Latah County and southeast Benewah County, at the headwaters of the Potlatch and Palouse rivers.

By the 1870s, homesteaders began to settle what is now Latah County, which was prized for its balance of rich agricultural land and historically, its stands of timber. The first sawmill in the area was established in 1876, six miles northeast of Moscow.

Latah County was carved from Nez Perce County, one of Idaho's original counties. A first attempt by early residents to create a separate county occurred on December 22, 1864, prompted by the difficulty of reaching the county seat in Lewiston from Moscow and environs, but the citizens of Lewiston, the county seat, resisted the process.⁷ "The people of Latah County made three separate efforts to perfect a county organization, but each time the opposition of Lewiston, county seat of Nez Perce County, was strong enough to prevent such action. In 1887 they appealed to Fred T. Dubois, then Idaho's delegate to Congress, for relief."⁸ President Cleveland approved an Act of Congress on May 14, 1888 that created Latah County, almost twenty years before the official formation of Deary.⁹

By the time of the 1890 census Latah County was the most populous county in Idaho with 9,176 residents.¹⁰ By the turn of the century, four railroads, the Northern Pacific, the Chicago, Milwaukee & St. Paul, the Oregon-Washington Railroad & Navigation Company, and of the Spokane & Inland Empire, served the county. It later gained a fifth with the arrival the Washington, Idaho & Montana Railroad (WI&M).¹¹ And while the 1920 *History of Idaho* did not mention Deary among the state's cities and towns or the unincorporated villages, it was noted in the 1903 history.¹² This same early history of Idaho stated that "the agricultural resources of the county are practically inexhaustible and all industrial interests must grow and keep pace with the gradual development of these resources."¹³

The Settlement and Early History of Deary

The WI&M, built to serve the Potlatch Lumber Company, reached Deary in 1906 and spurred the development of the town.¹⁴ While first settled in the 1880s and 1890s, it would be another 20-30 years before the town was incorporated on September 24, 1907.¹⁵ After the removal of the Potlatch Company to Potlatch and as the roads in the area improved, the town would become focused on Highway 8 or 2nd Avenue.

⁷ *An Illustrated History of North Idaho, embracing Nez Percés, Idaho, Latah, Kootenai and Shoshone Counties, State of Idaho.* Western Historical Publishing Company, 1903:581.

⁸ James H. Hawley, *History of Idaho: the gem of the mountains.* Chicago, IL: S.J. Clarke Publishing Co., 1920.

⁹ *An Illustrated History* . . . 1903:584.

¹⁰ Ray E. Osterberg, "The Settlement of Dry Ridge Troy-Deary, Idaho," *Latah Legacy*, Summer 1980:3.

¹¹ The Oregon Railroad and Navigation (O.R&N) Company had reached Moscow in 1885.

¹² Hawley, 1920.

¹³ *An Illustrated History* . . . 1903:584.

¹⁴ Anonymous, "A Brief History." On display at the Deary Garage, accessed April 2023.

¹⁵ Monroe, 2006:22.

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Among the town's early settlers were Joe and Lou Wells, African Americans from North Carolina, who arrived in 1889. The Wells traveled with brothers Frank and Crom Wells, whose family had enslaved Joe Wells prior to the Civil War.¹⁶ Joe became a successful farmer and lumberman, running multiple crews in the region.¹⁷ His wife was known for her hospitality.¹⁸ They also operated a lodging house.

A small farming community grew up in what is Deary today. Aside from the Wells, the city was settled primarily by Scandinavians from the Midwest, many of whom established homesteads under the Homestead Act of 1862. Under the provisions of this act, settlers could obtain 160 acres of land if they cleared 40 acres and lived on the property for five years. The pine timber on the land provided an early currency for the settlers, but the stands of giant pine trees soon attracted corporate interest, specifically the interest of the Weyerhaeuser Company, the parent company of what would become the Potlatch Lumber Company.

Following the arrival of the WI&M in 1906, Deary's development became focused on the railroad depot.¹⁹ The Deary Townsite Company laid out by the townsite and began selling lots the same day. F.C. McGowan and H.P. Henry, former employees of the Potlatch Lumber Company, managed the Deary Townsite Company and then opened the Latah County Realty Co. Ltd.²⁰ Businesses opened immediately, including a mercantile, a non-specified store, and a hardware store. The *Deary Enterprise* newspaper began publication in late 1908. The Deary Lumber Company opened its doors in 1909 and the Deary Clay Products Company, which manufactured bricks, was also in place. The Deary Grain Company served the farming community with a co-op and warehouses served the grain industry.²¹

By about 1915, a city directory boasted that the population of Deary was 300 and it was "the shipping and banking point for the rich farming country surrounding it."²² By that time the city had three general stores, a drug store, hardware store, harness shop, billiard room and confectionary, a three-story hotel, a millinery store, two large warehouses, a chop mill, barber shop, creamery, jewelry store, bank, livery barn, a weekly newspaper, two blacksmith shops, an electric light plant, two churches, a grade school, a Wells Fargo & Co. Express office, and the Deary Rural Telephone company. Five sawmills were located in the vicinity of the town.²³

¹⁶ "The Trees Grew Tall Photograph Collection," *University of Idaho*, <https://www.lib.uidaho.edu/digital/treesgrewtall/items/trees0.html>, accessed May 2023.

¹⁷ "Joseph "Joe" Wells Family History and Secondary Accounts Summary," *Cultural Resource Survey, Deary, Idaho*. Prepared for Latah County Historic Preservation Commission. Prepared by Preservation Solutions LCC, September 2019:32.

¹⁸ Op. cit., 2019:33.

¹⁹ Anonymous, "A Brief History." On display at the Deary Garage, accessed April 2023.

²⁰ Richard Waldbauer, "Deary, Carved Out of the White Pine Forest," *The Idahoan*, June 20, 1980.

²¹ Ibid.

²² R.L. Polk & Co., *Latah County Directory*, 1916-1917.

²³ Ibid.

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William Deary and the Potlatch Lumber Company

The city of Deary is named for William Deary, although he never lived there. Born on Allumette Island in Quebec, Canada on June 24, 1854, William Deary immigrated to the US in 1874.²⁴ Deary was the general manager for the Potlatch Lumber Company and the WI&M. He also had an affiliation with the Elk River State Bank and the Potlatch State Bank at the time of his death in 1913.²⁵ Deary began his career logging and “lumbering” in Michigan and Wisconsin. He had been working for the Northland Pine Company, which was founded by the Weyerhaeuser company, under the leadership of Frederick Weyerhaeuser, in the Midwest in 1899.²⁶ In 1900, Deary was living in Superior City, Wisconsin.²⁷ He left Wisconsin about that time, however, and moved west, where the stands of pine had not yet been depleted, as they had been in the Midwest. By 1901 he was based in Spokane and was still working for the Weyerhaeuser company.²⁸

Weyerhaeuser and Deary renamed the Northland Pine Company the Potlatch Lumber Company in 1903. Deary bought out the Codd Lumber Company mill in Colfax, Washington, and their timber lands in Latah County that same year.²⁹ He also bought a mill belonging to the Palouse River Lumber Company in Palouse, as well as timber lands, water rights, horses, and logging equipment for \$265,000.³⁰ By that time he was living in Palouse in Whitman County, Washington.

The company decided that the Potlatch Lumber Company mill would be constructed in Potlatch, where the company bought the town and mill site in 1905 and where land was more plentiful than at other sites the company looked at. That same year plans were completed for the mill by architect W.A. Wilkinson of Minneapolis, Minnesota. It was to be the largest white pine mill in the world and the last single-engine mill of its kind ever built.³¹ When the Potlatch Lumber Company moved its headquarters to Potlatch about 1907, Deary moved there with his family, where he remained for the rest of his life.

Deary directed the formation of the town of Potlatch, a company town that was developed primarily in 1906-07, when much of its housing was built for company employees.³² The

²⁴ U.S. Census, 1900.

²⁵ “Prominent Lumberman Dies at Potlatch,” *The Daily Star-Mirror* (Moscow, Idaho), May 7, 1913.

²⁶ “The Founders,” *Shaping a Company Town*, <https://sdryden3.wixsite.com/phscompanytown/the-founders>, accessed April 2023.

²⁷ U.S. Census, 1900.

²⁸ Ibid.

²⁹ “Construction Timeline,” *Shaping a Company Town*, <https://sdryden3.wixsite.com/phscompanytown/timeline>, accessed April 2023.

³⁰ “Construction Timeline,” *Shaping a Company Town*, <https://sdryden3.wixsite.com/phscompanytown/timeline>, accessed April 2023.

³¹ “Construction Timeline,” *Shaping a Company Town*, <https://sdryden3.wixsite.com/phscompanytown/timeline>, accessed April 2023.

³² Keith Petersen and Mary Reed, *Historic Resources of Potlatch (Partial Inventory: Architecture: National Register of Historic Places Inventory – Nomination Form*, November 26, 1985. Potlatch was designed to house 2,000 people. It included all the support businesses and organizations in addition to its residential section, which was in proximity to the Potlatch Mill. *A Walking Tour of the Potlatch Commercial District* (brochure), 2000. Courtesy Potlatch Historical Society. Accessed April 2023.

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Potlatch Lumber Company, and therefore William Deary, was also responsible for the development of the towns of Deary and Elk River. In Deary, he bought the Crooks and Blailock homesteads and traded a team of horses for the Roundtree property in order to site the new Deary townsite.³³ He also sited the towns of Princeton, Harvard, and Bovill, where he had obtained timber rights. The WI&M railroad linked all these towns beginning in 1906.³⁴

As general manager, Deary, who has been described as tenacious and enterprising, supervised the mill, the logging crews, the railroad, and construction and maintenance of the town of Potlatch.³⁵ He also managed the procurement of land and development of the other towns along the WI&M line, as well as the construction of the two mills controlled by the Potlatch Lumber Company. The town of Potlatch, in addition to the mill, remained under the ownership and control of the Potlatch Lumber Company until the 1950s.

Deary died on May 9, 1913 in Potlatch. The houses in the company-owned town were sold to private buyers in 1952. The Potlatch Lumber Company mill that Deary was responsible for closed in 1981 and the mill was dismantled in 1983. The WI&M railroad, which began operation in 1906, was sold in the 1960s (the depot in Potlatch is still extant, as is Deary's depot albeit in a different location). The government discontinued the mail contract with the railroad the same year that passenger service was discontinued.³⁶ Despite Deary's relatively short tenure in Latah and Whitman counties of just ten years, the company and by extension the town of Potlatch and the railroad he managed had a tremendous influence on the development of Latah County, including the town of Deary.³⁷

The Washington, Idaho & Montana Railway Company

The Potlatch Lumber Company acquired the rights-of-way for the WI&M by 1905 to build a railroad to access the rich timber lands of Latah County and connect them and the new towns to its mill at Potlatch. The engineers were also beginning to survey the route to Bovill from Potlatch. William Deary, general manager for the company, acquired homesteads in Deary on which to site the townsite and station for the WI&M railroad. Andrew Carlson established a way house in Deary and was catering to track crews; this site later became the Carlson Hotel.³⁸

³³ Osterberg, Ray E., "The Settlement of Dry Ridge Troy-Deary, Idaho," *Latah Legacy*, Summer 1980.

³⁴ Petersen, 1985, Section 8, Page 1.

³⁵ *A Walking Tour of the Potlatch Commercial District* (brochure), 2000. Courtesy Potlatch Historical Society. Accessed April 2023.

³⁶ "W.I.&M.Ry.Co," *The Potlatch Story*, Vol. II, No. III, September 1960:10.

³⁷ Hundreds of people attended his funeral in Potlatch, including a special delegation of the Knights of Columbus. Pallbearers, whom Deary selected during an earlier illness, were some of the most prominent men in the Potlatch company and other local businesses.³⁷ After his Catholic service, a special train took the funeral party to Spokane, where it was met by members of the local lodge of the Knights of Columbus and "scores" of friends. The mill and "every branch of business" in Potlatch was shut down during the funeral, as were all operations on the WI&M railway.³⁷ Deary is buried at Fairmount Cemetery in Spokane. A memorial dedicated to Deary was constructed in 1921 in front of the Athletic Club building in Potlatch, considered one of Potlatch's most significant buildings. It consists of two large boulders sourced from Elk Butte, close to Elk River, Idaho, and a bronze plaque to memorialize Deary's contributions to Potlatch and the county. "Hundreds Attend the Deary Funeral," *Palouse Republic*, May 16, 1913.

³⁸ Richard Waldbauer, "Deary, Carved Out of the White Pine Forest," *The Idahoan*, June 20, 1980.

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Rail construction began in 1905, overseen by Deary. The future WI&M constructed 20 miles of track in this first year, beginning in Palouse. By 1908, 49 miles of track were completed to Purdue, Idaho, just beyond Bovill. The utility of the new rail line was improved by the presence of the Spokane and Palouse Railway, which became the Northern Pacific, extended from Marshall, Washington to Genesee, Idaho, connecting to the WI&M at Palouse. In 1910, the Chicago, Milwaukee, St. Paul & Pennsylvania Railroad completed its branch line from St. Maries, Idaho to Elk River, Idaho, connecting to the WI&M at Bovill.³⁹

The primary purpose of the railroad was as a logging railroad, hauling this freight to the mill at Potlatch. Passenger service began in 1905 as well. The railroad also handled mail and provided shipping for produce and stock. The 1910s and 1920s were the heyday of the line, bringing logs to the Potlatch mill and sending cut lumber to other destinations. By the 1930s passenger traffic dwindled, "leading to the creation of the 'Bug' railcar, a self-propelled single car often with a gasoline engine, to carry mail, express, and the occasional passenger."⁴⁰ This was eventually replaced with "The Potlatcher" railcar, which was in service from 1938 until 1955, when passenger service was discontinued. In the 1960s the Potlatch Lumber Company sold the line to the Milwaukee Road.⁴¹

Criterion A Significance: Commerce

The development of the Deary Garage is representative of Deary's transition from a railroad town to one that catered to automobiles, specifically travelers along Highway 8 or 2nd Avenue in Deary. The building displays a strong orientation to the highway, with its location at the intersection of Main Street and 2nd Avenue, the orientation of its entry toward the intersection, its gasoline pumps clearly visible here, and its inviting interior stocked with supplies for the motorist. Its unique drive-through garage was also no doubt designed to be visible and appealing to the driver as well.

In the first years of the 20th century, car ownership in Idaho grew at a rapid pace, the result of improved roads and the increased affordability of automobiles.⁴² Idaho formed its State Highway Commission in 1913, which led to increased road building. In this year over 2,000 driver's licenses were issued in the state.⁴³ By 1918, Idaho's state highway system boasted 2,255 miles of roads, although only five were paved or oiled. But the only state route through Latah County

³⁹ "W.I.&M.Ry.Co," *The Potlatch Story*, Vol. II, No. III, September 1960:8.

⁴⁰ "History of The Washington, Idaho & Montana Railway," *WI&M Depot, Potlatch, Idaho*. [https://www.wimry.org/history/#:-:text=When%20the%20Milwaukee's%20Lines%20West,Northern%20Santa%20Fe%20\(BNSF\)](https://www.wimry.org/history/#:-:text=When%20the%20Milwaukee's%20Lines%20West,Northern%20Santa%20Fe%20(BNSF),), accessed May 2023.

⁴¹ *Ibid.*, The Milwaukee Road was eventually bought out by Burlington Northern, which became Burlington Northern Santa Fe, who ultimately sold it to WATCO. A segment of the line was passed on to the Washington & Idaho Railroad in 2007. Today this line serves only the Bennett Lumber Mill in Princeton, Idaho.⁴¹

⁴² *Cultural Resource Survey, Deary, Idaho*. Prepared for Latah County Historic Preservation Commission. Prepared by Preservation Solutions LCC, September 2019:12.

⁴³ *Cultural Resource Survey, Deary, Idaho*. Prepared for Latah County Historic Preservation Commission. Prepared by Preservation Solutions LCC, September 2019:12, quoting Rebecca Herbst's *Idaho Bridge Inventory, Volume 1, History*. Boise, ID: Idaho Transportation Department, 1983.

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was along what is now US Highway 95, which was characterized as “unimproved.”⁴⁴ In 1919, Deary’s bond issue for its own highway district was declared legal.⁴⁵ With this action almost all of Latah County was included in highway districts.

In these same decades, auto tourism was growing in Idaho and nationally. In the years after World War I, and by 1926, the highway department estimated that it accounted for more than 300,000 vehicles in Idaho each year.⁴⁶ Automobile clubs and other similar organizations were publishing touring routes and guidebooks. Town boosters, such as Chambers of Commerce and Deary’s Commercial Club, welcomed these opportunities.⁴⁷ Three trans-state highways developed in the 1910s and 1920s passed by Deary, if not through it, which increased auto tourism in general and attracted new automobile-related businesses and commercial development in the region.⁴⁸ “In 1926 the Idaho State Bureau of Highways initiated expanded highway programs to facilitate market-to-market commerce, as well as the transport of the mining and timber industries’ extracted raw materials and products.”⁴⁹ This may well have inspired Leo Beyer to build his new garage and gas station in Deary.

The Federal Highway Act, which was passed in 1921, additionally made monies available to aid state road and forest highway construction. “By 1922 the state highway system had expanded greatly and in Latah County new expansion included the routes between Moscow and Bovill and Moscow and Potlatch (present-day State Highway 8 and portions of State Highways 6 and 3) . . .”⁵⁰ A news item in 1922 announced that “satisfactory” progress was being made on the state highway from Moscow to Bovill, meaning that right-of-way had been cleared and bridges built.⁵¹

A Kendrick-to-Deary highway, planned in the late 1930s, benefitted from a rock crusher paid for by the Works Progress Administration (WPA).⁵² The routes through Deary were improved or graded but not paved, however. The 1937 atlas of Latah County showed that at that time the highway that extended from Moscow on Latah County’s west border, linking Moscow, Troy, Deary, Helmer, and Bovill, near the east border of Idaho, was a crushed rock highway. In a 1940 map of Idaho’s State Highway system, it also shows as crushed rock or gravel.⁵³ As truck transport continued to grow in places like Deary (and as rail transport became increasingly rare

⁴⁴ Op. Cit., 2019:12.

⁴⁵ “Deary’s Bond Issue Legal,” *The Spokesman Review*, October 31, 1919. With this almost all of Latah County was

⁴⁶ *Historic Survey of Roads In Idaho’s State Highway System Volume 1: Historic Context*. Prepared for the Idaho Transportation Department. Prepared by Mean & Hunt, December 2019:49.

⁴⁷ “Moscow Men Visit Deary in Automobiles,” *The Daily Star-Mirror* (Moscow, Idaho), July 15, 1912:1.

⁴⁸ Op. Cit., 2019:12.

⁴⁹ Ibid.

⁵⁰ *Cultural Resource Survey . . .* 2019:13.

⁵¹ “Latah County News Paragraphs,” *The Kendrick Gazette*, December 1, 1922:1.

⁵² “Loads Limited on Idaho Roads.” *The Spokesman Review*, February 12, 1938.

⁵³ Herbst, Rebecca, *Idaho Bridge Inventory, Volume 1, History*. Boise, ID: Idaho Transportation Department, 1983.

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for freight), state highways gradually improved.⁵⁴ The city's own website states that the highway from Deary to Bovill was built in 1957.⁵⁵

Developmental History of the Deary Garage

Historical maps show that the Deary townsite was composed of approximately 20 blocks.⁵⁶ The present site of the Deary Garage occupied lots 10 through 12 of Block 17 on the original townsite plat; the lots in this location are 25' wide and 130' deep. The 1921 Sanborn Fire Insurance map shows that at that time a general store occupied Lot 12 (addressed as 34 Main Street) at the corner of 2nd Avenue and Main Street, Lot 11 (33 Main Street) was undeveloped, and Lot 10 (32 Main Street) had a vacant building on the street and a shed at the back of the lot on the alley. The block between 2nd and 3rd Avenues along the west side of Main Street was the most densely developed area in the young city. As today, the two-story bank (bank at the ground level, lodgings above) occupied the corner of 3rd Avenue and Main Street. Although more sparsely developed, the block on the east side of Main Street included the Deary Garage (not the present building), the Deary Hotel, and the Highway Hotel, among other buildings. The two-story Highway Hotel at the corner of 2nd Avenue and Main Street was opposite today's Deary Garage.

Portions of downtown Deary burned in October 1923. "The big fire struck in early October 1923. It burned all the buildings fronting the west side of Main Street below the bank . . . The bank stood slightly alone [and was the only brick building along this stretch], and it was possible to keep it splashed with water. By this means, the bank was saved."⁵⁷ The 2019 survey of Deary states that the pool hall, drugstore, mercantile, hardware store, post office and confectionary burned. All the buildings but the bank building on this side of the street were wood. The next Sanborn Fire Insurance map, dated 1928, shows the town as it was rebuilt, including four new brick buildings on the west side of Main Street and no wood buildings.⁵⁸ It also shows the new brick Deary Garage, one of the few remaining buildings from Deary's period of reconstruction following the fire.

The *Pacific Builder and Engineer*, a Seattle-based publication, published a call for bids to construct a new Deary Garage submitted by Leo Beyer shortly after the fire in its June 28, 1924 issue.⁵⁹ The 1928 Sanborn map shows the new 1926 Deary Garage, which was labeled as "Garage" and "Rep'g." Information on the maps shows that the garage had the capacity for 30 cars.⁶⁰ Other notes indicate that it had a concrete floor, a truss roof, plastered walls, and was heated by a stove, possibly with a gasoline engine. Gas pumps were located under a canopy at

⁵⁴ The WI&M depot in Deary closed about 1946.

⁵⁵ "History," *City of Deary, Latah County, Idaho*, <http://www.dearyidaho.com/history.html>, accessed February 2023.

⁵⁶ Note that the townsite map shows 24 blocks but several are bisected by the railroad right-of-way, which passes through the center of town from the northwest to the southeast.

⁵⁷ "The Trees Grew Tall Photograph Collection," *University of Idaho*, <https://www.lib.uidaho.edu/digital/treesgrewtall/items/trees0.html>, accessed May 2023.

⁵⁸ Sanborn Fire Insurance maps, 1921, 1921 updated to 1928.

⁵⁹ "Advertisements for Bids," *Pacific Builder & Engineer*, June 28, 1924.

⁶⁰ The earlier Deary garage, which shows on the 1921 and 1928 Sanborn maps, had the capacity for 11 cars. On the 1928 map it was called "auto storage."

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the building entrance. A separate area at the entrance (where the showroom would later be located, and which is today part of the restaurant) held auto supplies. A separate area in the northeast corner held batteries. The purpose of a small square-plan addition on the back of the building is unknown.

When constructed the Deary Garage was a Red Crown Service Station. Red Crown was an affiliate of Standard Oil. The 1920s was the greatest expansion period for the Standard Oil Red Crown Service Station and was a time of unprecedented growth and profitability.⁶¹

Founder Leo Otto Beyer

Leo O. Beyer built the garage at 307 Main Street, referred to here as the Deary Garage. He may also have built the earlier Deary Garage, which is now the site of the Deary City Park. Leo Otto Beyer was born on March 21, 1882 to Fred and Albertine Beyer, who were German immigrants, in Zumbrota, Minnesota. In about 1892, at age 10, he moved to Latah County from Rochester, Minnesota with his parents. In 1900 he lived west of Deary in Big Bear with his parents and brother Paul.⁶² In 1906 he was attending college and working for the Hazelwood Company in Spokane and in 1907 he was working as a bookkeeper for the Hazelwood Company.⁶³

In 1910, Beyer was living in what was now called Bear Creek with his parents, brother, and new wife, Ella, whom he had married on December 29, 1906 and with whom he had a child named Clarence in 1909. He was working as a bookkeeper at Dalkena Lumber Company. According to ledgers found at the Deary Garage, the Deary Garage business was initiated about 1912.⁶⁴ By 1916 Beyer was working as the manager of the Deary Garage.⁶⁵ This early Deary Garage was a one-story building with a capacity for 11 cars, located at 72 Main Street (the location of Deary City Park today).⁶⁶

Beyer would continue to manage a garage, first at the location above and later at his new garage (the subject building) for the rest of his life. On his 1917-1918 draft card, his occupation was listed as "garage man." In July 1919, Leo married Lila Hokes Bolton of New York in Spokane. On the marriage license he listed himself as a "garage man." In the 1920 census he was self-employed as the proprietor of a garage. In the 1930 census he was listed as the manager of a garage, while his wife Lila H., was the bookkeeper. In the 1940 census he listed himself as an automobile dealer in Deary. In his 1942 draft card he listed himself as self-employed with the Beyer Motor Co. in Deary. He died on March 8, 1945, at age 62. In his death certificate his occupation was listed as a garage owner.

In the later years of Beyer's ownership of the Deary Garage it was a Ford dealership that he called the Beyer Motor Co. It would appear that Beyer was serious about his work. An advertisement for the Detroit-based Michigan State Auto School in *Popular Mechanics* shows

⁶¹ L. Robert Puschendorf, *National Register of Historic Places Registration Form, Standard Oil Red Crown Service Station*, April 2004.

⁶² U.S. Census, 1900.

⁶³ R.L Polk & Co., *Latah County Directory*, 1906, 1907.

⁶⁴ Anonymous, "A Brief History." On display at the Deary Garage, accessed April 2023.

⁶⁵ R.L Polk & Co., *Latah County Directory*, 1916-1917.

⁶⁶ Sanborn Fire Insurance map, 1921.

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that Leo Beyer attended courses there. His testimony states, “The Training I received in your Welding Department and your special electrical course has paid me larger dividends per dollar, than any money I ever spent.”

Subsequent Owners

Two photographs of the Deary Garage are on display in the building today. A photograph from the late 1940s/early 1950s shows that the name on the front canopy was “Wylie Motor Co.” On the south façade is a sign that says, “Atlas Tires & Batteries.” A pole sign in this location advertises Mobil gas. Another sign on the east façade, facing Main Street, is not legible. A later photograph from the mid-to-late 1950s shows that the building is now the “Deary Service Station.” The sign on the south façade notes “Mobile Tires & Batteries.” The pole sign is still present; there is no sign on the east façade.

The history of the building posted in the Deary Garage tells of the subsequent owners after Beyer’s death. “In 1946 the business became Wylie Motors, owned by Claude and Anna Wylie.⁶⁷ From the mid-1950s through 1979 it was Lee and Parsons, owned by Esten Less and Pat Parsons. Bill Anderson took over in 1979 and the business became Deary Automotive. Over the years it [the building] housed a barbershop and an antique store, but eventually it became useful only for storage. . . Due to the quality of the original building, a solid shell remained when we began renovations in November, 2015.”⁶⁸ Today the building is owned by a small consortium of owners who have an artisanal cheese manufacturing facility in the west half of the building and a bakery and pizzeria in the east half of the building.

Criterion C Significance: Architecture

The Deary Garage is an excellent example of an early automobile-oriented building in Deary and a very good example of local commercial architecture. As a commercial building type, the Deary Garage is a one-part commercial block, meaning that it has only one story and is characterized as a simple box with a decorated façade that is urban in character.⁶⁹ A Western False Front building, such as seen in early Deary, would have been a common one-part block type in the late 19th and early 20th centuries. A later one-part commercial block would have a typical storefront. That is, it would have a centered front door flanked by storefront or large plate glass windows; all would be topped by transom windows. The area above the door and window would be reserved for the decorative aspects of the building and signage. Decorative features might be a shaped parapet, a heavy cornice with dentil course or other embellishments, or signage that might be integrated as part of the design features of the building. A canopy over the sidewalk or serving as a marquee might also be integrated with the design of the building.

The 1926 Deary Garage is not a typical commercial building, in that it has a corner entry. However, the arrangement of the storefront, with its central door, storefront windows, and transoms, is typical of a commercial building such as this. It was popular in the 1910s and

⁶⁷ In the 1950 census listing for Claude E. Wylie, he listed himself as an owner-operator and the business as a garage Service Station. He also stated that he worked 80 hours a week.

⁶⁸ Anonymous, “A Brief History.” On display at the Deary Garage, accessed April 2023.

⁶⁹ Richard Longstreth, *The Buildings of Main Street*. Walnut Creek, CA: Alta Mira Press, 2000 (1987):54.

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1920s to incorporate contrasting brick patterns in the area above the storefront, which the Deary Garage features in modest form. The Deary Garage additionally features diamond shapes in a light-toned brick as embellishments along the southeast façade and east elevation. Architectural historian Richard Longstreth describes changes to storefront design that began to occur in the 1920s as follows.

By the 1920s . . . efforts emerged to make the one-part commercial block in suburban areas more ornamental and visually harmonious with its domestic surroundings.⁷⁰ The abundance of automobiles and corresponding traffic congestion also fostered the concept that low-density commercial development was preferable . . . Some of the resulting changes are minor, such as use of a few decorative embellishments. In other instances, the shift in character may be quite pronounced, with large, picturesque elements modifying the basic configuration.⁷¹

As a use type, the Deary Garage most closely resembles a gas station with an integrated garage.⁷² Historically, the front portion of the Deary Garage was used for selling auto supplies.⁷³ Its large storefront windows would ensure that these wares were visible to the passing motorist or gas customer. The building's prominent location on the main highway through Deary in the center of town, and its orientation toward this main intersection, is consistent with this use.

The gas pump for consumer use was developed in 1905. The style of gas pump seen in Figure 25, showing the Deary Garage about 1926, has been described as follows. "These units were of the visible-measure type commonly in use by the 1920s. Gas was pumped up from an underground tank until the glass graduate was filled to the desired level. The gas was then released through the base and flowed by force of gravity into the tank of an awaiting car. Pump makers introduced this innovation to visually reassure motorists that they got what they paid for."⁷⁴

By 1910, the demand for automobiles and corresponding demand for gas had increased dramatically and oil companies were seeking to profit from this consumer market.⁷⁵ Gas began being sold in conjunction with other commercial buildings, such as garages, and older uses such as blacksmiths and liveries were converted to businesses that catered to the automobile-driving public. "By the time of World War I, stations were mushrooming along the open road, vacant lots lining the taxpayer strip, Main Street gap sites, and even the lawns of homes fronting residential boulevards at a rate of 1,200 or more a year."⁷⁶

⁷⁰ The same might be said of small towns such as Deary.

⁷¹ Longstreth, 2000:62.

⁷² The Deary Garage was later used as an automobile showroom and a garage, to which its design was well-suited as well. It is not known when the gas pumps were removed.

⁷³ Sanborn Fire Insurance map, 1921 updated to 1928.1928.

⁷⁴ Chester H. Liebs, *Main Street to Miracle Mile, American roadside Architecture*. Baltimore, DE: The Johns Hopkins University Press, 1995 (1985):96.

⁷⁵ Liebs, 1995:95.

⁷⁶ Liebs, 1995:97.

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By the 1920s, however, combining gas pumps with other uses and placing them along “Main Street” was causing traffic congestion and potential safety hazards.⁷⁷ At this point purpose-built gas stations, such as the Deary Garage, began to be more common. The Deary Garage’s unknown designer solved the problem of getting the automobile out of traffic while it was filling up by providing a place to pull off the street by the corner entry. In addition, the garage featured a canopy to shelter gas attendants and customers, another feature of the modern gas station.

While free-standing gas stations continued to evolve, gas stations that also served as garages like the subject property began to be built to meet the demand for automobile service. “By the late 1920s, the gas station was evolving into a hybrid of filling station and repair garage, and the neighborhood service station was born.”⁷⁸ By the 1930s, it was common for the new gas station/garage to serve several functions, such as selling other goods, including an office for the attendant, providing restrooms, etc. Gas pumps began being sited on an island, away from garage bay doors and away from customer entries (the latter was not the case with the Deary Garage). By the 1930s, this became the norm, a model that is still in place today. Another trend that occurred during the Depression was that ‘gas stations’ began selling auto supplies. “. . . as gas revenues slackened during the Depression, oil companies became increasingly dependent on selling tires, batteries, and accessories . . . as a way of generating greater income per station. Built more as eye-catching shelters than as showcases, most older stations simply were not designed with the floor area, lighting, or display windows to exhibit these new sources of revenue effectively.”⁷⁹

Gas stations with service bays continued to evolve in the 1930s. Oil companies began developing prototype gas stations with garage service and branding their architecture and signage to look new and up to date. While it is not known whether the Deary Garage was conceived as a gas station first or a garage that also sold gasoline, its location and functional layout neatly combined these two functions and reflected some of the most modern thinking to date, reflecting the state of the art in the late 1920s and early 1930s. It additionally has the appearance of an attractive, urban building befitting its location in central Deary.

Summary

The Deary Garage is significant under NRHP Criteria A and C, at the local level of significance. It is eligible under Criterion A in the area of COMMERCE for its association with Deary’s transition from its beginnings as essentially a railroad town for the Potlatch Lumber Company to a town that catered, through this building, to the motoring public as automobile ownership rose and the state highway system, along which the property is located, improved. Established by Leo. O. Beyer, the Deary Garage was the town’s first modern, purpose-built, all-inclusive garage, with automobile repair, auto supplies sales, and gasoline sales for the motorist. Built in the aftermath of a 1923 fire that burned most of the business district, the garage also represents Deary’s commercial development during the ensuing period of reconstruction. The Deary Garage is eligible under Criterion C in the area of ARCHITECTURE for its design, as a notable

⁷⁷ The architectural historian Chester Liebs notes that congestion was not as large a problem in smaller towns or more rural locations as it was in a larger city or suburban location.

⁷⁸ Liebs, 1995:102.

⁷⁹ Liebs, 1995:104.

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local example of early automobile-oriented architecture that reflects national trends and as an attractive local commercial vernacular building that is representative of its time and retains integrity. The period of significance is 1926-1945, beginning with the building's completion and ending with Beyer's death.

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Deary Garage
Name of Property

Latah County, Idaho
County and State

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Deary Garage
Name of Property

Latah County, Idaho
County and State

Previous documentation on file (NPS):

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # _____
- recorded by Historic American Engineering Record # _____
- recorded by Historic American Landscape Survey # _____

Primary location of additional data:

- State Historic Preservation Office
 - Other State agency
 - Federal agency
 - Local government
 - University
 - Other
- Name of repository: _____

Historic Resources Survey Number (if assigned): N/A

10. Geographical Data

Acreage of Property < 1 acre

Use either the UTM system or latitude/longitude coordinates

Latitude/Longitude Coordinates

Datum if other than WGS84:
(enter coordinates to 6 decimal places)

- | | |
|------------------------|----------------------|
| 1. Latitude: 46.799722 | Longitude: 116.56111 |
| 2. Latitude: | Longitude: |
| 3. Latitude: | Longitude: |
| 4. Latitude: | Longitude: |

Or

Deary Garage
Name of Property

Latah County, Idaho
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UTM References

Datum (indicated on USGS map):

NAD 1927 or NAD 1983

- | | | |
|----------|----------|-----------|
| 1. Zone: | Easting: | Northing: |
| 2. Zone: | Easting: | Northing: |
| 3. Zone: | Easting: | Northing: |
| 4. Zone: | Easting: | Northing: |

Verbal Boundary Description (Describe the boundaries of the property.)

The boundary is the limits of Assessor Parcel Number RPD1280017010A. It is also represented by Lots 10-12, Block 17, of the Original Townsite.

Boundary Justification (Explain why the boundaries were selected.)

The boundary is commensurate with the Assessor Parcel Number for this urban lot.

11. Form Prepared By

name/title: Diana J. Painter, PhD, Principal Architectural Historian
organization: Painter Preservation
street & number: 3518 N C Street
city or town: Spokane state: WA zip code: 99205
e-mail: dianajpainter@gmail.com
telephone: 707-763-6500
date: May 15, 2023

Deary Garage
Name of Property

Latah County, Idaho
County and State

Additional Documentation

Submit the following items with the completed form:

- **Maps:** A **USGS map** or equivalent (7.5 or 15 minute series) indicating the property's location.
- **Sketch map** for historic districts and properties having large acreage or numerous resources. Key all photographs to this map.
- **Additional items:** (Check with the SHPO, TPO, or FPO for any additional items.)

Figure Log

Figure 1: Regional location map, map of Latah County

Figure 2: Topographical map, Deary Quadrangle, 7.5-minute series, 2020

Figure 3: Deary Garage, neighborhood context

Figure 4: Deary Garage building with parcel boundary, 45.799722 N, 116.56111 W

Figure 5: Sketch plan 2015

Figure 6: Sketch plan with photo locations

Figure 7: Site plan with photo locations

Figure 8: Joe and Lou Wells, early settlers in Deary, ca 1900

Figure 9: Joe Wells and a logging crew

Figure 10: Deary, Idaho in 1906, Potato Hill in the distance

Figure 11: Deary Townsite map, 1907, with Deary Garage site outlined

Figure 12: William Deary at his desk, 1907

Figure 13: Washington, Idaho & Montana railroad depot in Deary, 1907

Figure 14: Rail excursion timetable from the Palouse to Boville, 1907

Figure 15: Latah County State Bank at Main Street and 3rd Avenue, 1908

Figure 16: Typical storefront in Deary, ca 1908

Deary Garage
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- Figure 17:** Deary townscape, railroad depot to right, 1909
- Figure 18:** A State Highway near Deary
- Figure 19:** Deary townsite and Deary Home and Orchard Tracts, 1910
- Figure 20:** Deary streetscape with false front buildings, 1911
- Figure 21:** Sanborn Fire Insurance map showing Deary in 1921, with existing and future site of Deary Garage
- Figure 22:** Aftermath of 1923 fire on Main Street in Deary
- Figure 23:** Sanborn Fire Insurance map showing Deary in 1928, with new Deary Garage
- Figure 24:** Leo Beyer attends The Auto Center in Detroit
- Figure 25:** Deary Garage ca 1926
- Figure 26:** Deary Garage in the late 1940s
- Figure 27:** Deary ca 1955 showing Deary Garage in context

Photographs

Submit clear and descriptive photographs. The size of each image must be 1600x1200 pixels (minimum), 3000x2000 preferred, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

Photo Log

Name of Property: Deary Garage

City or Vicinity: Deary, Idaho

County: Latah

State: Idaho

Photographer: Diana Painter; Idaho SHPO

Date Photographed: March 5, 2023; August 16, 2023

Deary Garage
Name of Property

Latah County, Idaho
County and State

Description of Photograph(s) and number, include description of view indicating direction of camera:

- Photo 1 of 20: ID_LatahCo_DearyGarage_0001
South façade, entry, and east façade, looking northwest
- Photo 2 of 20: ID_LatahCo_DearyGarage_0002
Entry at 2nd Avenue and Main Street, looking northeast
- Photo 3 of 20: ID_LatahCo_DearyGarage_0003
South façade, looking north
- Photo 4 of 20: ID_LatahCo_DearyGarage_0004
Windows on south façade, typical
- Photo 5 of 20: ID_LatahCo_DearyGarage_0005
Rear (west) façade, looking east
- Photo 6 of 20: ID_LatahCo_DearyGarage_0006
Rear (west) façade, sign detail
- Photo 7 of 20: ID_LatahCo_DearyGarage_0007
Restaurant at entry, looking southeast
- Photo 8 of 20: ID_LatahCo_DearyGarage_0008
Looking from restaurant to dining room, facing north
- Photo 9 of 20: ID_LatahCo_DearyGarage_0009
Dining room, looking east
- Photo 10 of 20: ID_LatahCo_DearyGarage_0010
Dining room, looking west
- Photo 11 of 20: ID_LatahCo_DearyGarage_0011
Looking from dining room into restaurant, looking south
- Photo 12 of 20: ID_LatahCo_DearyGarage_0012
Truss detail, dining room, looking southwest
- Photo 13 of 20: ID_LatahCo_DearyGarage_0013
Truss detail, dining room, looking west

Deary Garage
Name of Property

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-
- Photo 14 of 20: ID_LatahCo_DearyGarage_0014
Restaurant, looking west toward gift shop
- Photo 15 of 20: ID_LatahCo_DearyGarage_00015
Gift shop, looking south
- Photo 16 of 20: ID_LatahCo_DearyGarage_0016
Gift shop, looking east towards restaurant
- Photo 17 of 20: ID_LatahCo_DearyGarage_0017
Cheese kitchen, looking west
- Photo 18 of 20: ID_LatahCo_DearyGarage_0018
Looking at south end of Main Street, east side, Deary Garage to left
- Photo 19 of 20: ID_LatahCo_DearyGarage_0019
Looking at north end of Main Street, east side, facing southwest
- Photo 20 of 20: ID_LatahCo_DearyGarage_0020
Refurbished and moved train depot (410 Main Street)

Paperwork Reduction Act Statement: This information is being collected for nominations to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.). We may not conduct or sponsor and you are not required to respond to a collection of information unless it displays a currently valid OMB control number.

Estimated Burden Statement: Public reporting burden for each response using this form is estimated to be between the Tier 1 and Tier 4 levels with the estimate of the time for each tier as follows:

- Tier 1 – 60-100 hours
- Tier 2 – 120 hours
- Tier 3 – 230 hours
- Tier 4 – 280 hours

The above estimates include time for reviewing instructions, gathering and maintaining data, and preparing and transmitting nominations. Send comments regarding these estimates or any other aspect of the requirement(s) to the Service Information Collection Clearance Officer, National Park Service, 1201 Oakridge Drive Fort Collins, CO 80525.

United States Department of the Interior
National Park Service

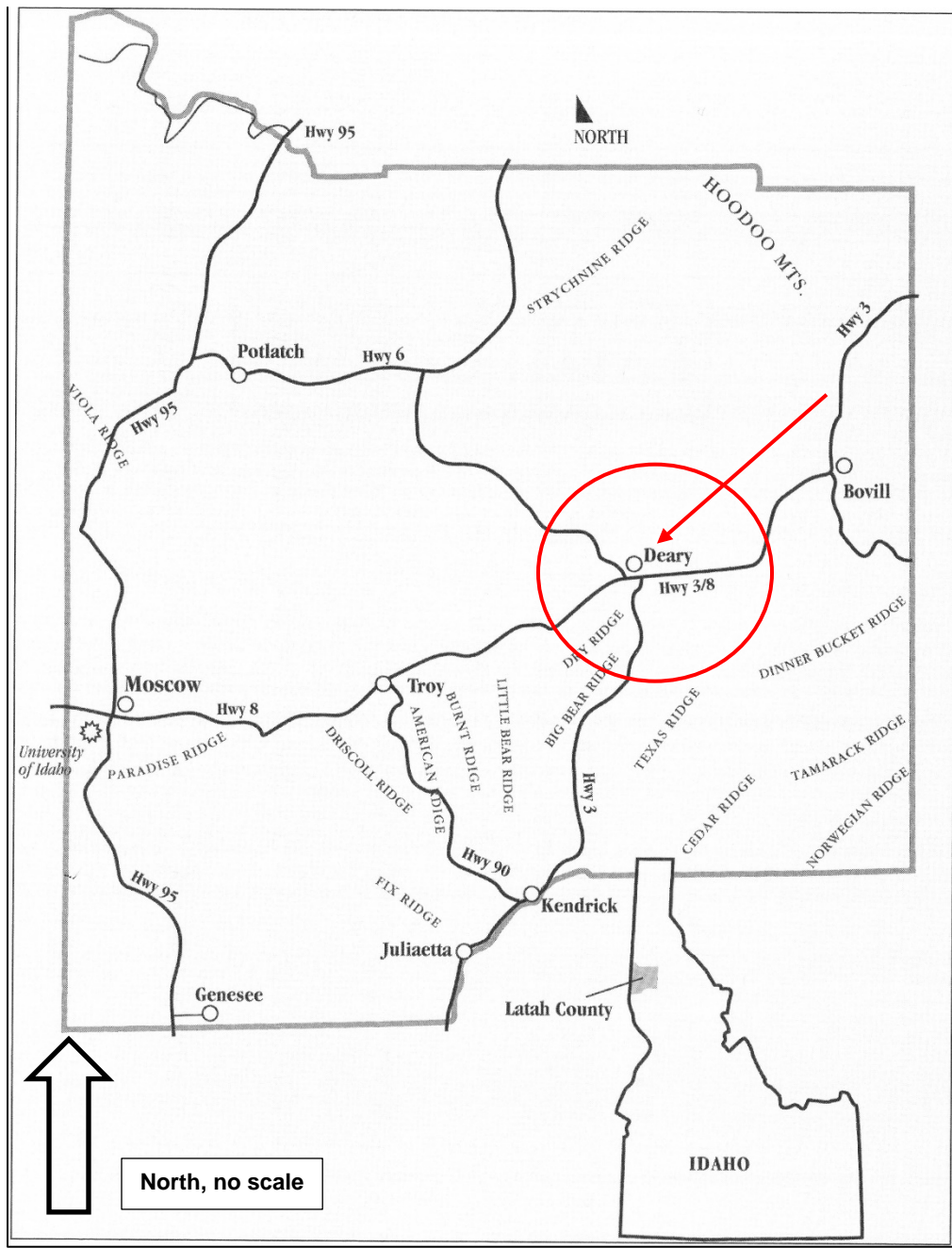
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N/A
Name of multiple listing (if applicable)

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Figure 1: Regional location map, map of Latah County



Source: Latah County, 2006

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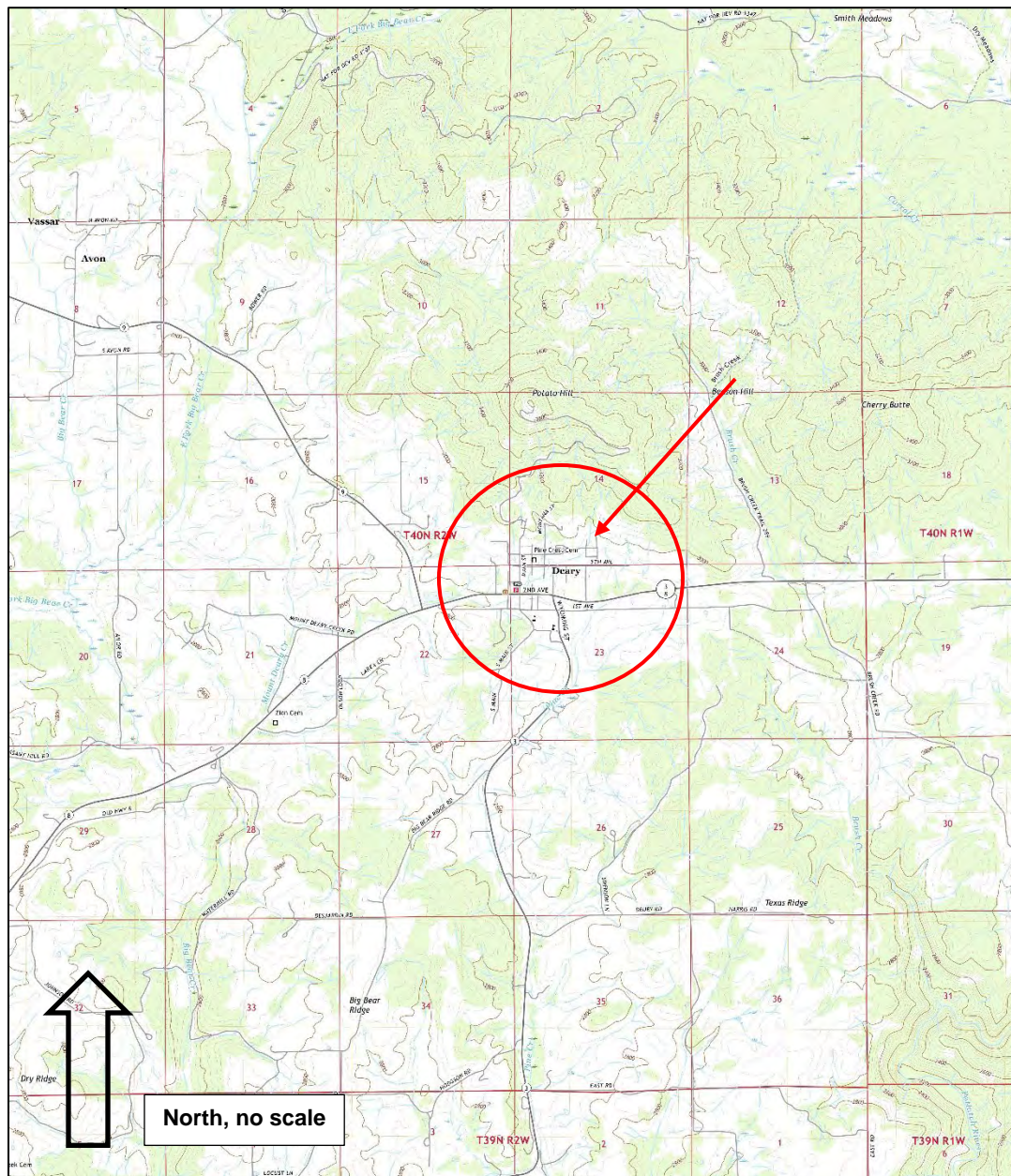
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Figure 2: Topographical map, Deary Quadrangle, 7.5-minute series, 2020



Source: USGS topographical maps

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N/A
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Figure 3: Deary Garage, neighborhood context



Source: Google maps

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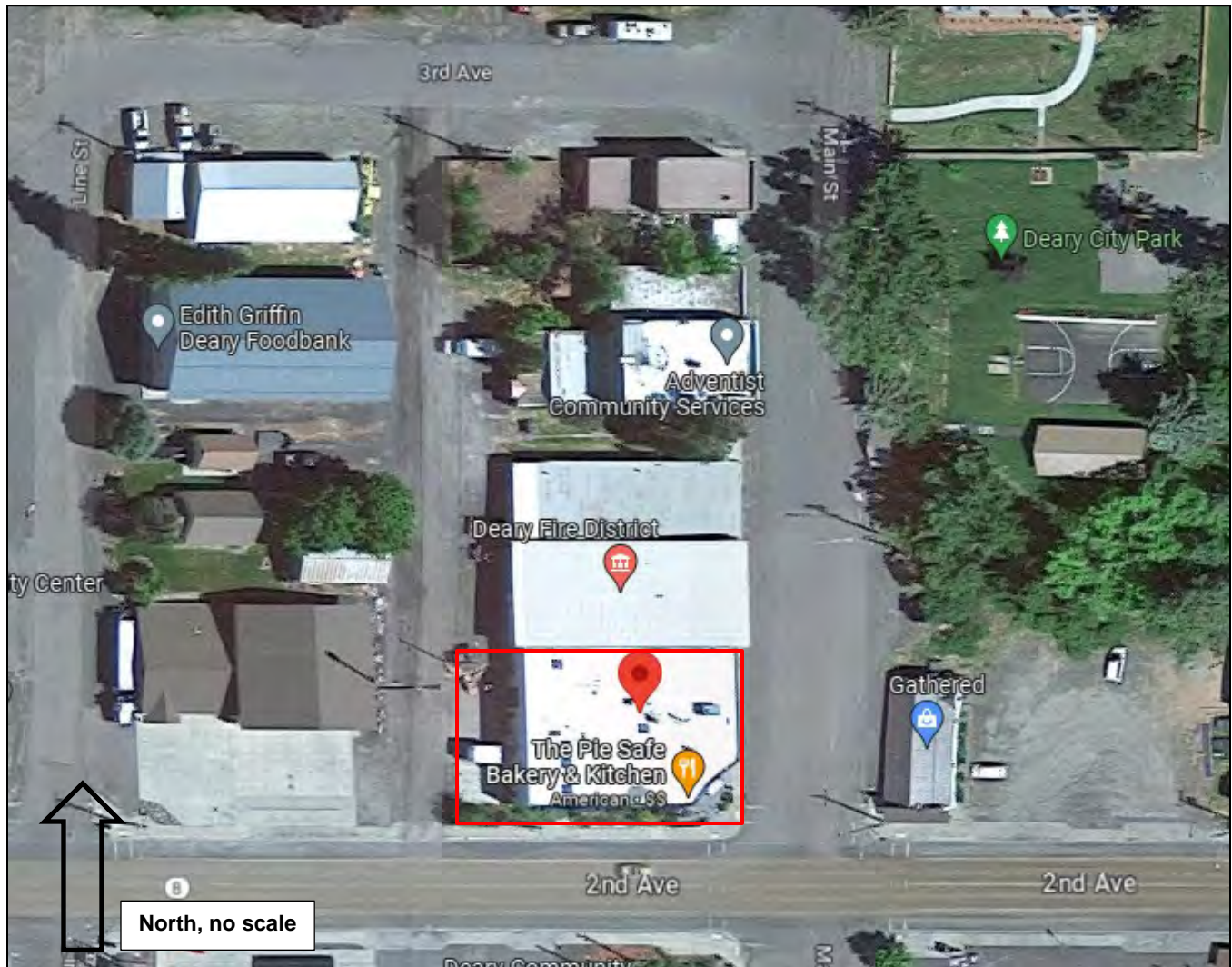
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Figure 4: Deary Garage building with parcel boundary, 45.799722 N, 116.56111 W



Source: Google maps

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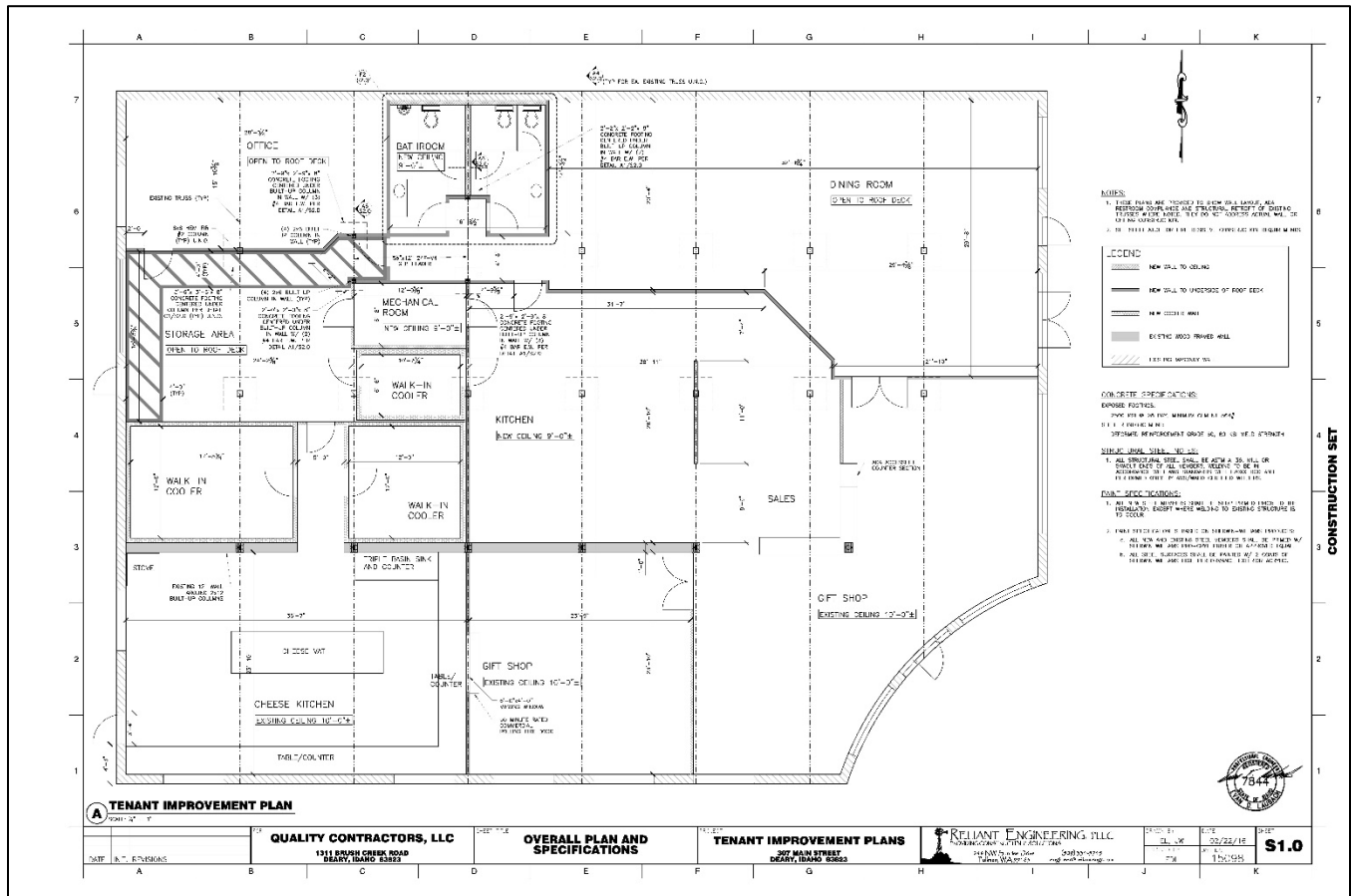
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Figure 5: Sketch plan, 2015



Source: Quality Contractors, LLC

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N/A

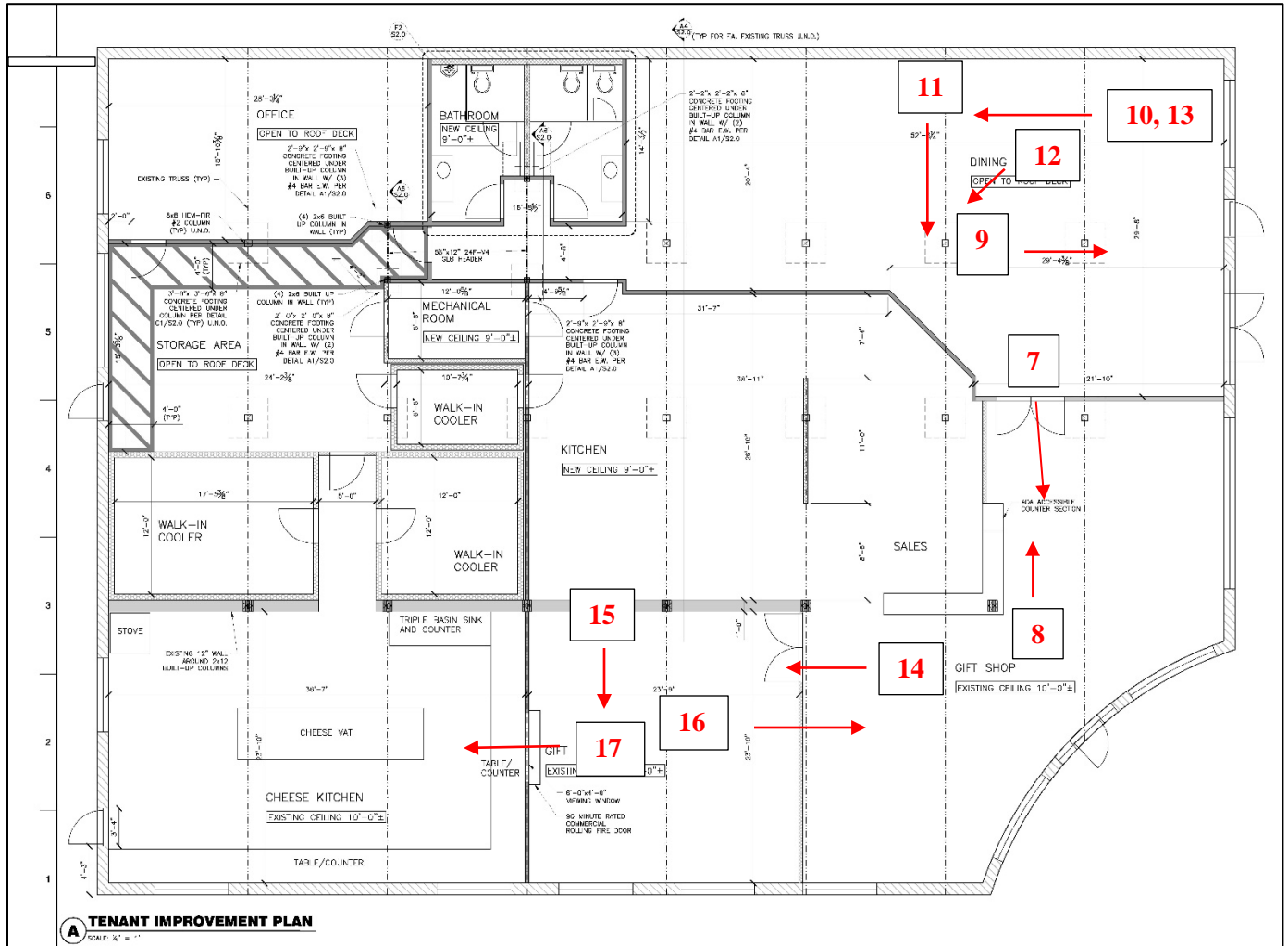
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Figure 6: Sketch plan with photo locations



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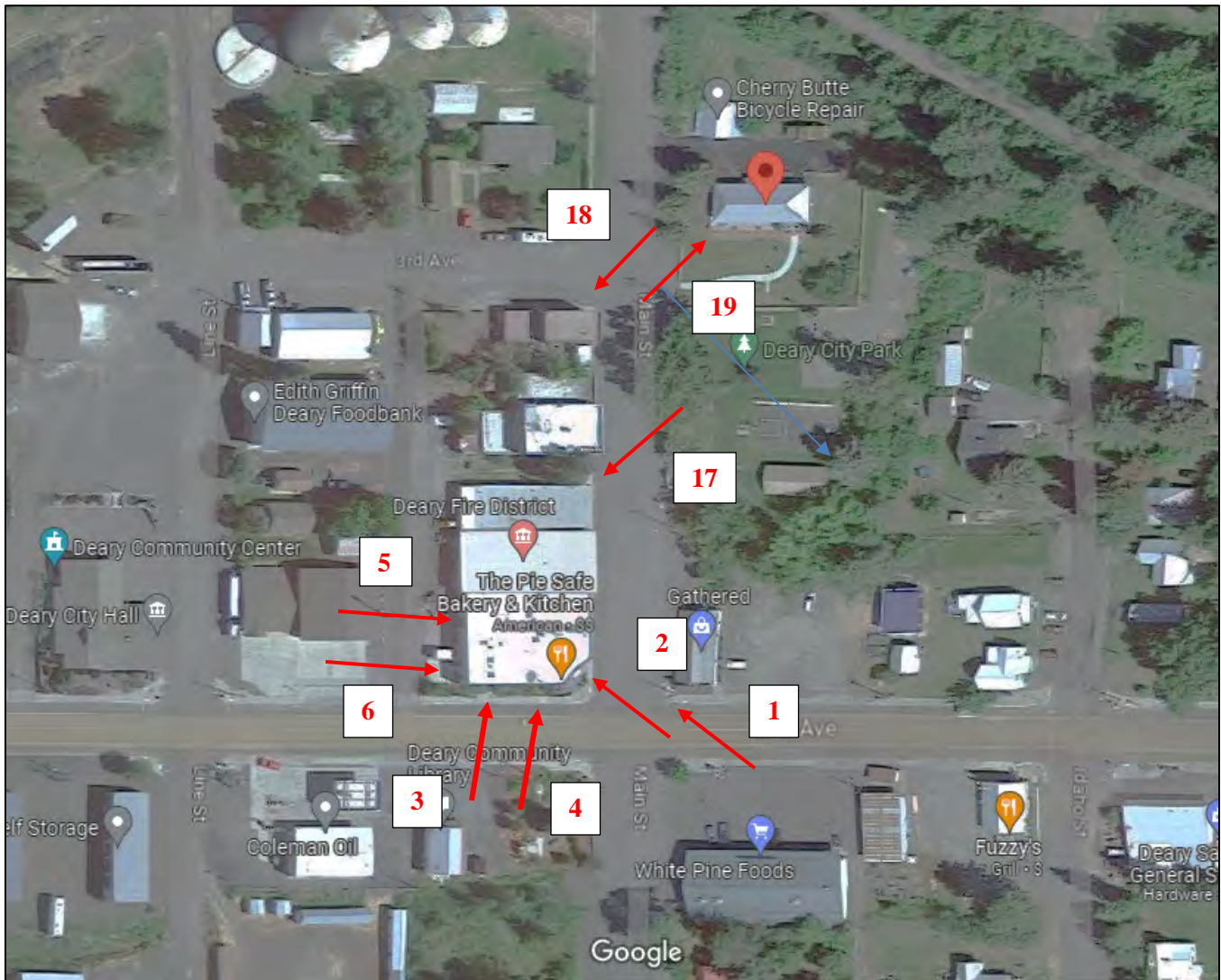
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Figure 7: Site plan with photo locations



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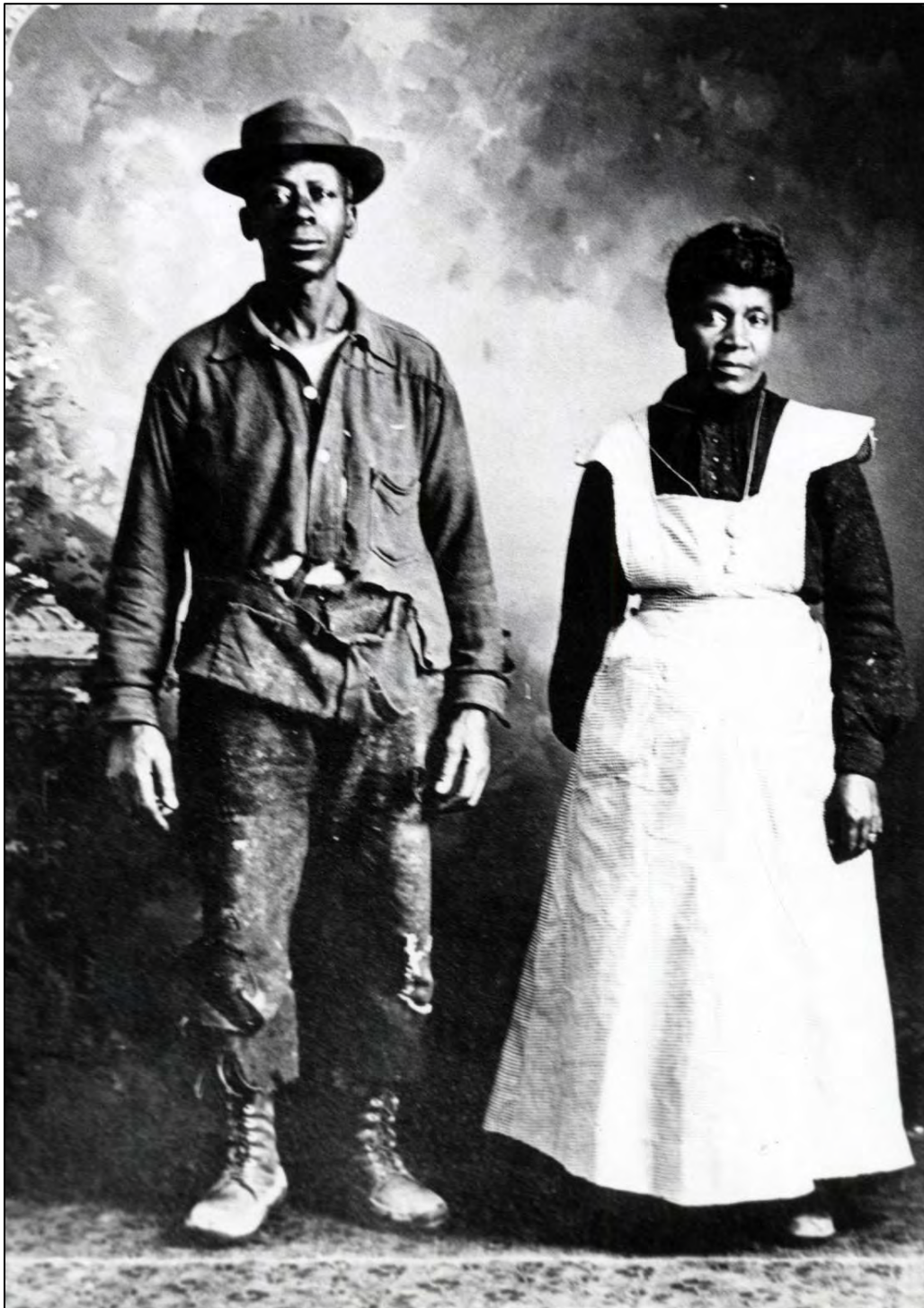
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Figure 8: Joe and Lou Wells, early settlers in Deary, ca 1900



Source: University of Idaho Special Collections & Archives

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Figure 9: Joe Wells and a logging crew



Source: University of Idaho Special Collections & Archives

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Figure 10: Deary, Idaho in 1906, Potato Hill in the distance



Source: University of Idaho Special Collections & Archives

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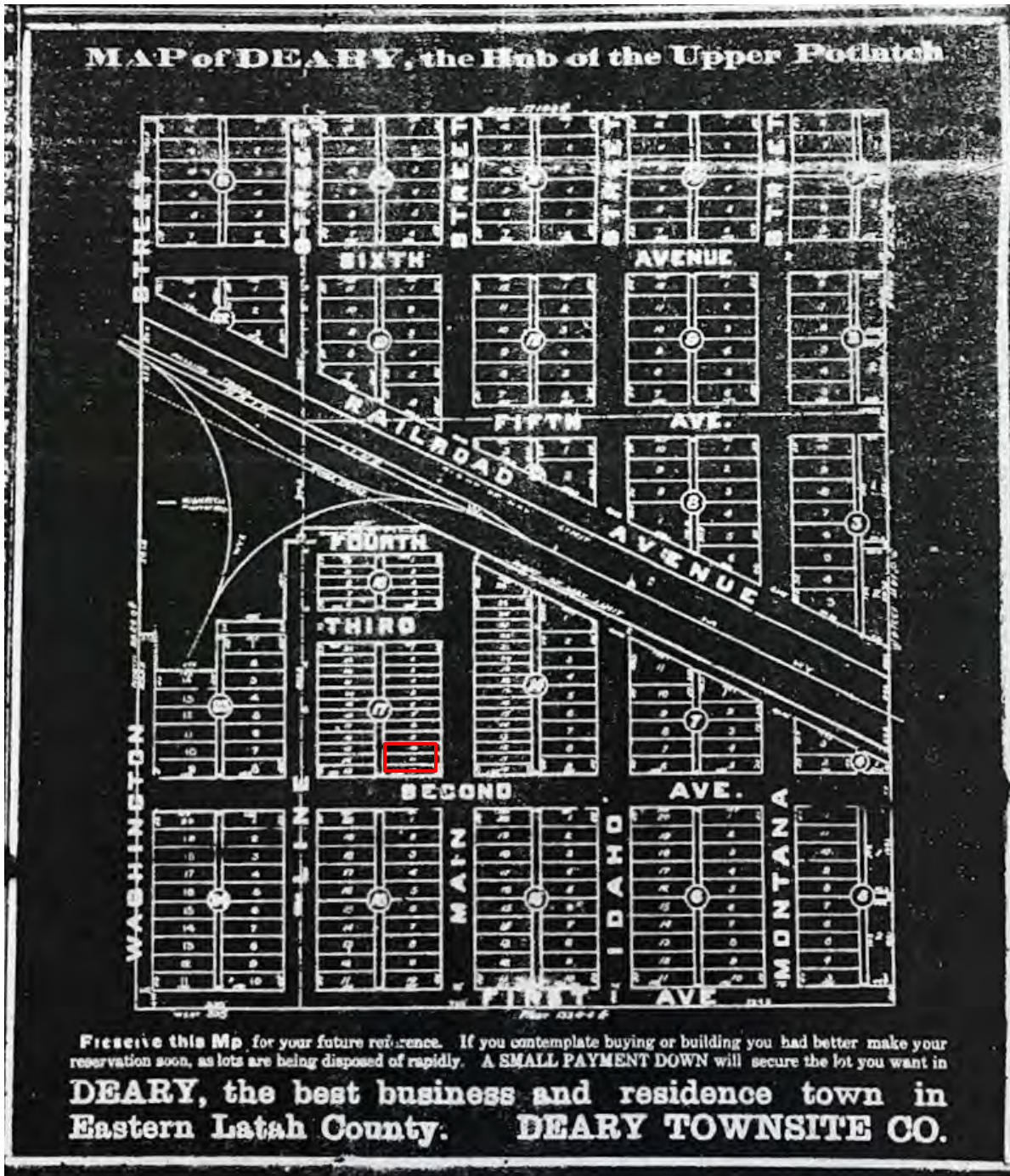
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Figure 11: Deary Townsite map, 1907, with Deary Garage site outlined



Source: Potlatch Historical Society, Potlatch-WI&M Heritage Center

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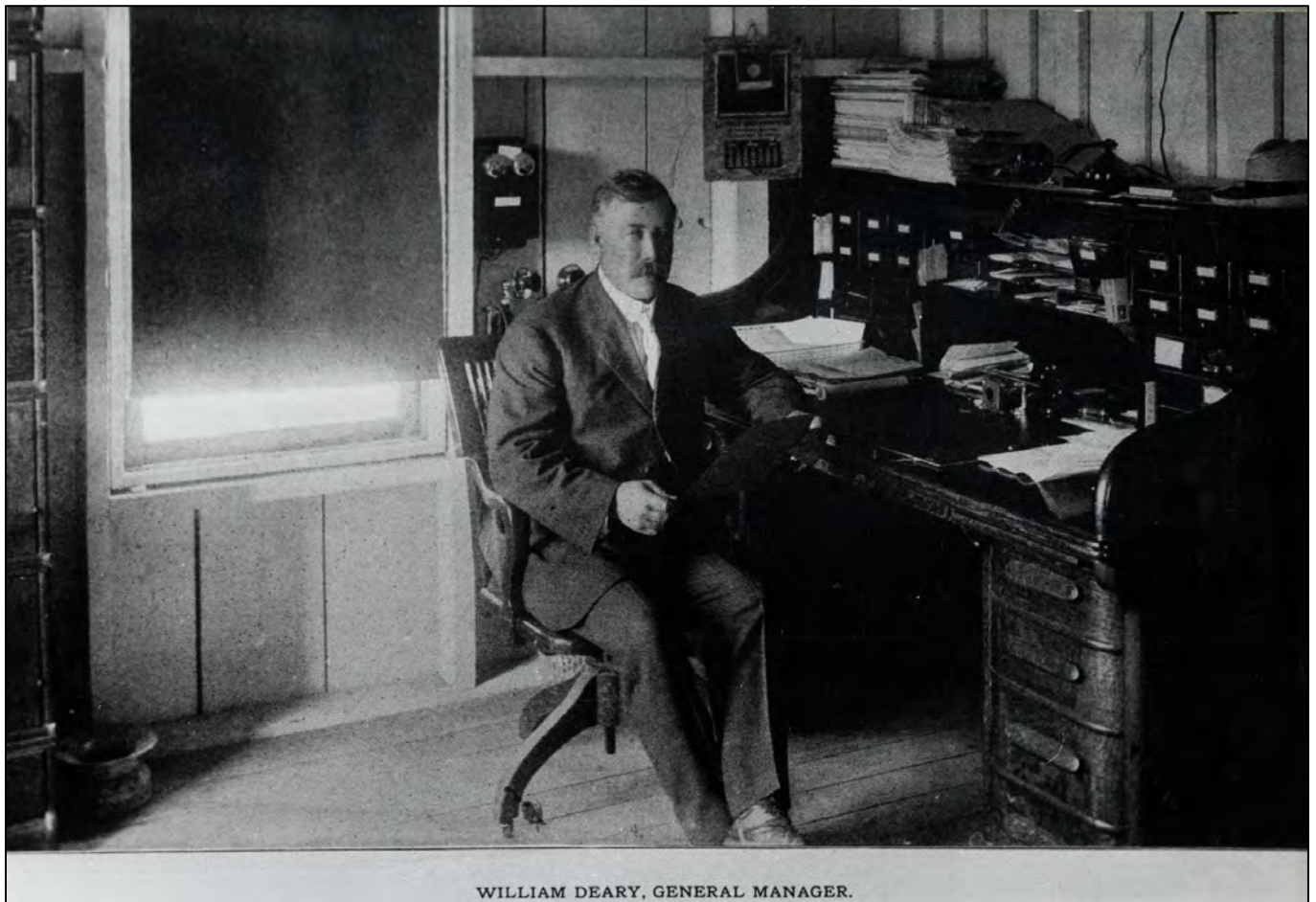
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Figure 12: William Deary at his desk, 1907



Source: University of Idaho Special Collections & Archives

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Figure 13: Washington, Idaho & Montana railroad depot in Deary, 1907



Source: University of Idaho Special Collections & Archives

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Figure 14: Rail excursion timetable from the Palouse to Bovill, 1907

EXCURSION

PALOUSE to BOVILL

The Washington, Idaho & Montana will run its first through train to Bovill

SUNDAY, OCTOBER 13, 1907

This will be a special excursion train affording the people of Palouse and vicinity the opportunity to see the finest timber and agricultural section of Idaho. The weather man predicts a fine day; special open top observation cars will be provided.
A fine chance to see the "white pine forest." Good time guaranteed.

On return trip a stop of one hour will be made at Deary

Very Low Rates

Round Trip Fares

From	From
Palouse.....\$1.50	Summit..... 85 cents
Kennedy Ford.....\$1.30	Stanford..... 75 cents
Potlatch.....\$1.25	Avon..... 65 cents
Princeton.....\$1.15	Deary..... 50 cents
Harvard.....\$1.00	Cornell..... 30 cents

Children under twelve, Half Fare

TIME TABLE

Distance from Palouse	East Bound	STATION	West Bound
0	8:30 a. m.	PALOUSE	5:03 p. m.
7	8:52 a. m.	Kennedy Ford	4:40 p. m.
11	9:00 a. m.	POTLATCH	4:33 p. m.
11	9:10 a. m.	POTLATCH	4:23 p. m.
14	9:20 a. m.	Princeton	4:13 p. m.
20	9:36 a. m.	Harvard	3:58 p. m.
25	9:51 a. m.	Summit	3:45 p. m.
29	10:00 a. m.	Stanford	3:35 p. m.
31	10:05 a. m.	Avon	3:30 p. m.
34	10:20 a. m.	DEARY	3:15 p. m.
34	10:25 a. m.	DEARY	2:15 p. m.
40	10:40 a. m.	Cornell	1:55 p. m.
47	11:05 a. m.	Bovill	1:30 p. m.

WILLIAM DEARY, GEN. MANAGER.

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Figure 15: Latah County State Bank at Main Street and 3rd Avenue, 1908



Source: University of Idaho Special Collections & Archives

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N/A

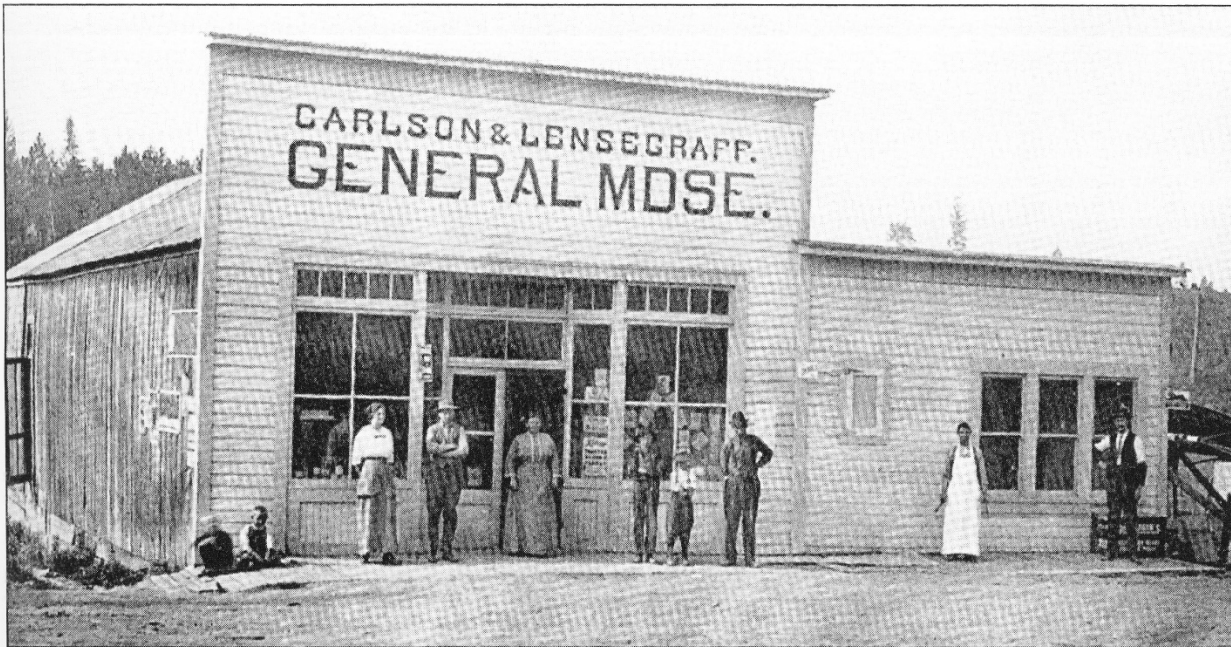
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Figure 16: Typical storefront in Deary, ca 1908



Source: Latah County, 2006

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Figure 17: Deary townscape, railroad depot to right, 1909



Source: University of Idaho Special Collections & Archives

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N/A

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Figure 18: A State Highway near Deary



Source: University of Idaho Special Collections & Archives

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National Park Service

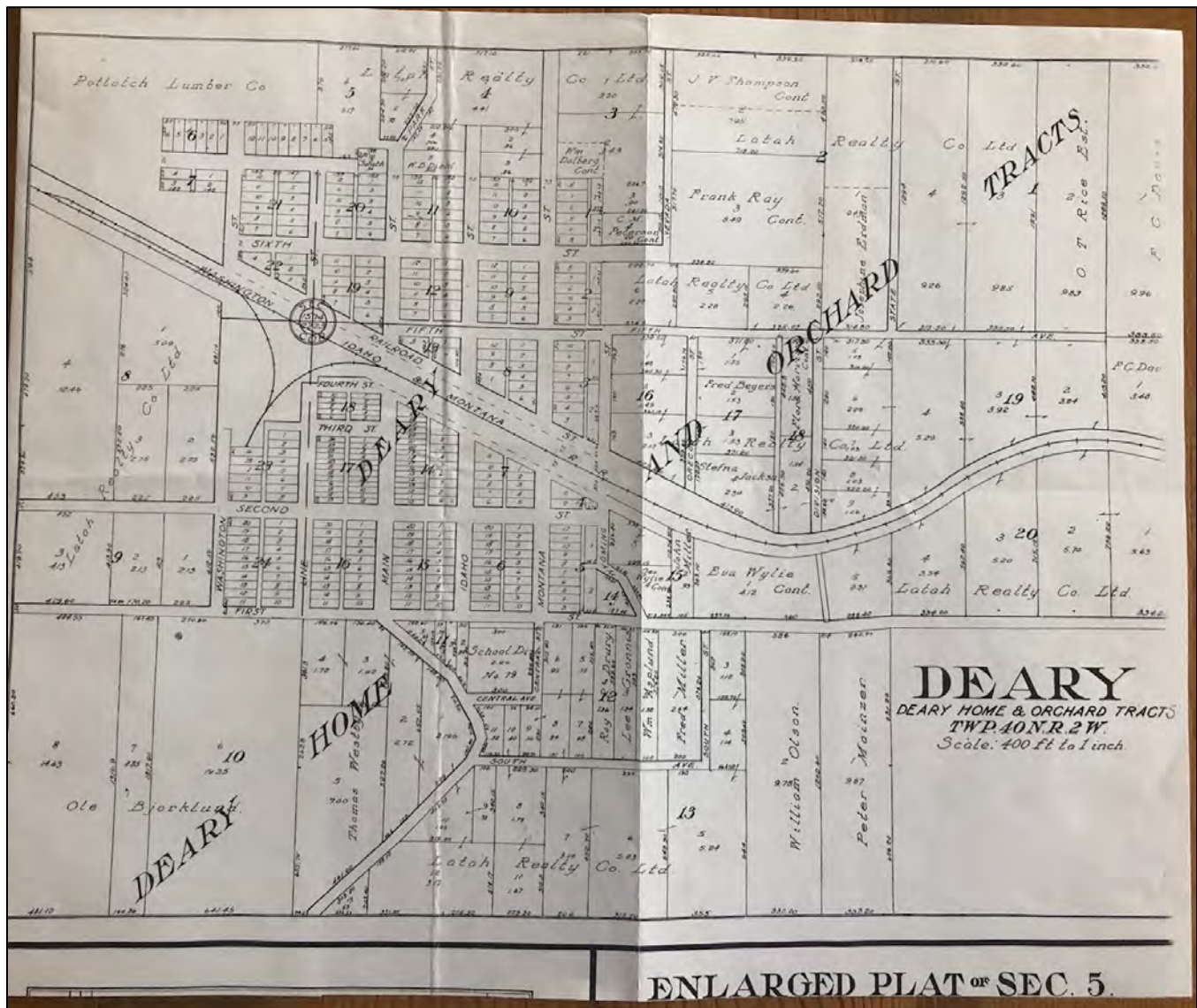
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Figure 19: Deary Townsite and Deary Home and Orchard Tracts, 1910



Source: Potlatch Historical Society, Potlatch-WI&M Heritage Center

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Figure 20: Deary streetscape with false front buildings, 1911



Source: University of Idaho Special Collections & Archives

United States Department of the Interior
National Park Service

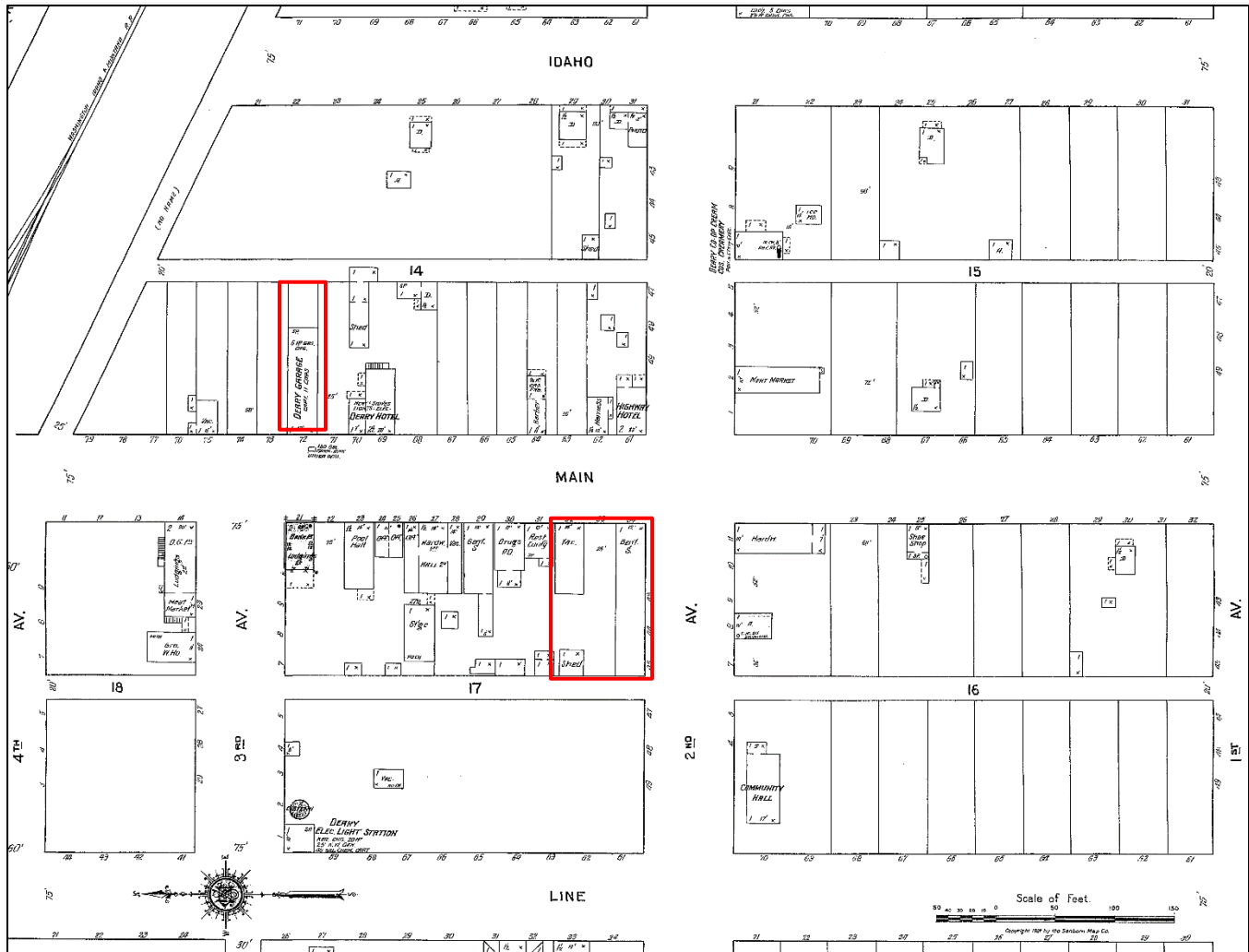
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Figure 21: Sanborn Fire Insurance map shows Deary in 1921, with existing (upper left) and future (lower right) site of Deary Garage



Source: Seattle Public Library

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Figure 22: Aftermath of 1923 fire on Main Street in Deary



Source: University of Idaho Special Collections & Archives

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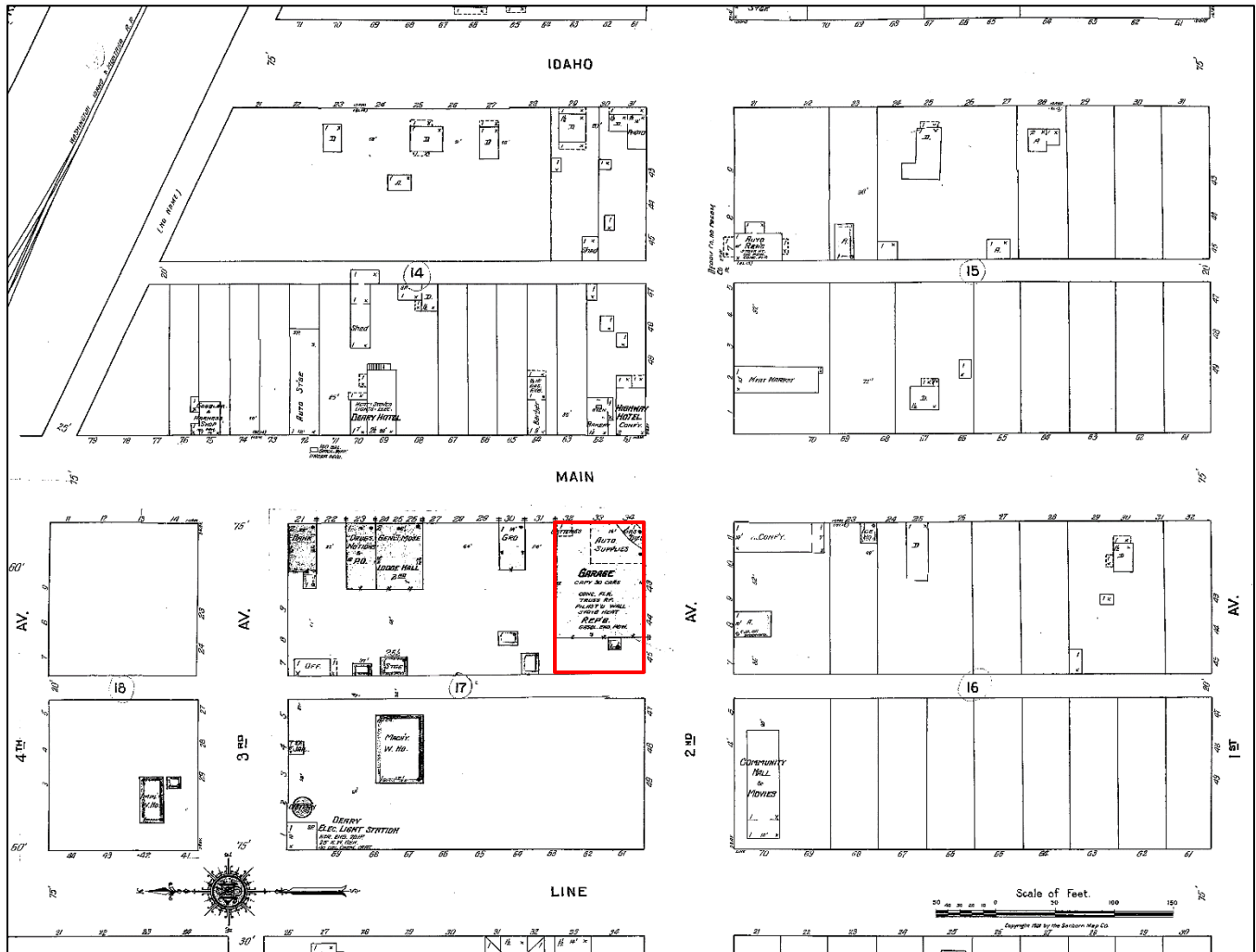
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Figure 23: Sanborn Fire Insurance map shows Deary in 1928, with new Deary Garage



Source: Seattle Public Library

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N/A
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Figure 24: Leo Beyer attends The Auto Center in Detroit

POPULAR MECHANICS ADVERTISING SECTION 61

the Auto Center

Study and Learn Now—it is the thoroughly trained men who make the big successes in the auto truck and tractor business. M. S. A. S. training will make you independent the rest of your life. You will secure in a few weeks of pleasant, practical study and training what hard times or adverse conditions can never take from you. Trained heads and hands always get first call on big money and in business.

Original and Only Factory Endorsed School

When the Michigan State Auto School first started in 1910, we called in the big men of the industry. Detroit was first in this tremendous field. It has maintained the lead until now 73% of all motor vehicles are made right here and a total of 94% within 100 miles. The big factories gave us their hearty co-operation at first and have continued to do so, for they needed the highest type of men and this school was giving the thorough training needed. Write to any automobile factory, business house or bank in Detroit and ask them about us.

Visit Detroit Factories

Training in Detroit at the M. S. A. S. gives you another advantage enjoyed by no other school any place. Regular trips are made weekly or oftener through the factories. The Packard, Paige, Jewett, Ford, Fordson, Studebaker, Lincoln, Cadillac, Hudson, Essex, Maxwell, Chalmers, this is only a partial list and the big engine and accessory factories are also visited. You meet the factory heads, study factory methods of assembly and repair. This is a big advantage over all the rest and cannot be enjoyed except in Detroit at the M. S. A. S.

Factory Trained Instructors

Many of our instructors had early training in the big factories—they practically grew up in the business. Those who did not, have gone into the factory methods in co-operation with the factory heads and give you the result of this intense research and training.

Instructors Long With School

Most of our instructors at the heads of departments have been with us a long time. Some, three years; others, eight years. These men have developed in their departments and are able to pass on their experience to you.

Complete Auto, Truck and Tractor Course

Our complete course includes training in the construction, operation, upkeep and repair of automobiles, trucks, tractors, farm lighting plants and stationary engines of every kind. Basically the instructions are alike, but we teach how to apply the knowledge to all motor vehicles. We have a large general garage for actual garage practice. We own a farm with all types of tractors for operation and upkeep practice. The course includes thorough training in automotive electrics with actual practice in overhauling, rebuilding all types of starting, lighting and ignition systems. You get everything from A to Z. We educate the head and train the hand in a thorough, complete way.

Tire Repairing

We have a separate course in this branch of the auto business—the best equipment and instructors train you. Our graduates are very successful.

Battery Repair

This important branch of the industry is also taught in a separate and complete course which equips our graduates to immediately go into this big profit business.

Welfare Department Gets Good Rooms

Our Welfare Department is every student's friend. The day you arrive, it locates you in a nice room and directs you to good table board. It helps you with personal matters, tells you where your church is located, helps you get the best care and attention. The M. S. A. S. Welfare Department is known around the world.

Mail the coupon for catalog and Auto News—all FREE—or, better still, just jump on the train and come right to the school.

Brazing and Welding

There is a big field for this work in every industry, for it is the best and quickest means of repairing all metals. This is in a separate course and is thoroughly taught.

Machine Shop

Garages are getting this branch of repair into use and trained men in the automobile industry find big paying jobs easily. This separate course is also complete and thorough.

GRADUATES' LETTERS

J. H. Goodwin, New Jersey state distributor for the Crow-Elliott and Jackson cars, writes:
"I thought I knew something about an automobile when I went there, but after going through and looking back, I find I did not know anything. The benefit I have derived from the tutoring that I received cannot be described, as I owe all my success to it. M. S. A. S. graduates are always given preference by me."
Mr. Goodwin's address is care Goodwin Automobile Co., 228 Central Ave., Newark, N. J.

L. C. Beyer, of Deary, Idaho:
"The training I received in your Welding Department and your special electrical course has paid me larger dividends per dollar, than any money I ever spent."
COURTESY OF HONORARY PRESIDENT, M. S. A. S. writes: "I was the first one of the good jobs, due to his M. S. A. S. training. He writes: "I graduated from the M. S. A. S. in 1920. I returned home and got a job immediately and worked three months as mechanic and then became foreman of the Dodge and Liberty garage at a salary of \$300 a month, which job I still hold."
Thomas' address is 1822 Woodmont Ave.

H. S. Brooley, 2340 E. 55th St., Cleveland O., rose from a job as shipping clerk at \$12.00 a week, to a garage owner making \$500 a month. He now has a fine place in Cleveland.

F. W. Johnson, Cleveland, was a "pencil pusher" at \$12.00 a week before coming to the M. S. A. S., now is an auto electrician averaging more than three times that.

J. R. Navarro, is Service Manager for the Dodge Bros. branch, Roncesvalles, W. Va. He says:
"I have worked with men who have had fifteen years experience, but my schooling has put me up with them or ahead of them."

J. B. Nelson, Worcester, N. Y., took our course when 17 years old, now has five men working for him.

E. R. Greve, Fowler, Cal., writes:
"Last year's business bought up a nice little home here, and we have started buying another for this year."

Geo. H. Purvis, 0111 Charlevote Ave., writes:
"Am now experimental electrician with the Packard Motor Car Company. Here we get about everything there is in auto electrical work. I give your school all the credit."

Johr Gralak, Chrisholm, Minn.—
"I am in business for myself. My wages average from \$350 to \$400 a month. Before I went to your school I hardly made enough for my family to live on."

SEND THIS COUPON TODAY!

Michigan State Auto School
308 Auto Bldg., Detroit, Mich.

Gentlemen: Send me your Catalog and Auto News FREE.

Name

Street or R. F. D.

Town

State

Automobile School

"Endorsed by all Leading Manufacturers"

Detroit, Mich., U. S. A.

Source: Popular Mechanics

United States Department of the Interior
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Deary Garage

Name of Property

Latah County, Idaho

County and State

N/A

Name of multiple listing (if applicable)

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Figure 25: Deary Garage ca 1926



Courtesy Sharon Harris

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Figure 26: Deary Garage in the late 1940s



Beyer Motor Co. (DEARY-01), view NW, late 1940s

Courtesy Brush Creek Creamery exhibit

Source: Cultural Resource Survey, Deary, Idaho, 2019

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Figure 27: Deary ca 1955 showing Deary Garage in context



View SE of Deary, no date (c.1955)
Courtesy Brush Creek Creamery exhibit

Source: Idaho Historic Sites Inventory Form, Deary Garage,

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Photo 1 of 20: South elevation, entry, and east elevation, looking northwest



Photo 2 of 20: Entry at 2nd Avenue and Main Street, looking northeast

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Photo 3 of 20: South elevation, looking north



Photo 4 of 20: Windows on south elevation, typical

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Photo 5 of 20: Rear (west) elevation, looking east



Photo 6 of 20: Rear (west) elevation, sign detail

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Photo 7 of 20: Restaurant at entry, looking southeast

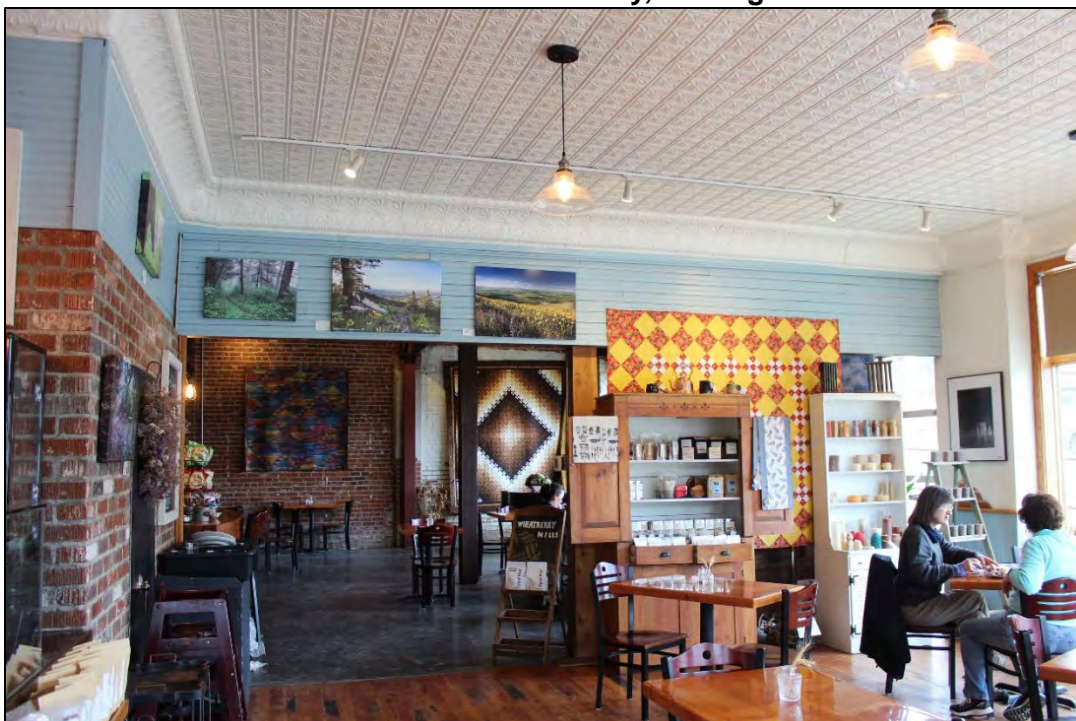


Photo 8 of 20: Looking from restaurant to dining room, facing north

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Photo 9 of 20: Dining room, looking east



Photo 10 of 20: Dining room, looking west

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Photo 11 of 20: Looking from dining room into restaurant, looking south



Photo 12 of 20: Truss detail, dining room, looking southwest

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Photo 13 of 20: Truss detail, dining room, looking west



Photo 14 of 20: Restaurant, looking west toward gift shop

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Photo 15 of 20: Gift shop, looking south



Photo 16 of 20: Gift shop, looking east towards restaurant

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Photo 17 of 20: Cheese kitchen, looking west



Photo 18 of 20: Looking at south end of Main Street, east side, Deary Garage to left

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Photo 19 of 20: Looking at north end of Main Street, east side, facing southwest



Photo 20 of 20: Refurbished and moved train depot (410 Main Street)